

# 1 Rosetta Street, Halton Hills

## Planning Justification Addendum Report



Application for Official Plan and Zoning By-law Amendment

LEV LIVING™

Prepared for 1 Rosetta Street Inc.  
by Arcadis  
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# 1 Introduction

This Planning Justification Addendum is to be read in conjunction with the original Planning Justification Report (“PJR”) dated June 30<sup>th</sup>, 2022, prepared in support of the Official Plan Amendment and Zoning By-law Amendment applications for the lands located at 1 Rosetta Street, and 6 and 8 St. Michael Street, Halton Hills (herein collectively referred to as the “subject lands”). The formal comments in response to the application were received in the Fall/Winter of 2022 from the Town of Halton Hills departments, Region of Halton, and external agencies. Various comments were also received from the public during the Statutory Public Meeting held on February 13<sup>th</sup>, 2023.

Comments received for consideration include but are not limited to the inclusion of commercial space within the proposed development, providing a pedestrian access for future residents to access the GO Station lands to coordinate with Metrolinx on their future plans for the GO Station, a request to extend St. Michael’s Street to John Street, traffic circulation and compatibility concerns. Comments and responses from the consultants are itemized in the Comment Response Matrix included within the submission. Following the receipt of formal comments, the conceptual site plan was revised to address comments received from the Town, which are detailed in **Section 3** of this report.

The original PJR provided a fulsome assessment of the proposed development. Many of the planning policy discussions would mirror the planning justification found within the original report. Therefore, for the purposes of this addendum report, the detailed planning analysis will focus on addressing the Town and Agency comments and provide an overview of the updated supporting technical reports.

In summary, this addendum provides the following information:

- A summary of the public consultation history to date,
- A description of the changes to the proposed development,
- The revised draft Official Plan and Zoning By-law Amendments,
- A response to Town, Agency and Public comments.

To permit the proposed development, an Official Plan Amendment (“OPA”), Zoning By-law Amendment (“ZBLA”), Draft Plan of Condominium and Site Plan Applications continue to be required to implement the development proposal. A Separate Pre-Consultation Meeting will be needed for the review of the Site Plan and Draft Plan of Condominium applications.

# 2 Public Consultation

Prior to the submission of the initial application, an introductory meeting with the Regional and Town Ward Councillor was conducted on June 22<sup>nd</sup>, 2022, to introduce the development concept and discuss high-level matters. The matters discussed related to the park space facilities and access, art and commemoration, urban design that addresses the heritage, construction timing and impact as well as the connectivity to the GO Station. Following this meeting, the first submission for Official Plan and Zoning By-law Amendment was made on June 30<sup>th</sup>, 2022.

A Statutory Public Meeting was held on February 13<sup>th</sup>, 2023. The agent, owners, traffic, and engineering consultants were in attendance to answer questions. Some of the public concerns that were brought up during the meeting included: height, density, infrastructure phasing and sun/shadow impacts. These questions and other written comments and responses are provided in the itemized Comment Response Matrix attached included within the submission.

Following the election in October 2022, a second introductory meeting with Mayor Lawlor and Town Ward Councillors was conducted on March 23<sup>rd</sup>, 2023, to introduce the updated development concept, and discuss high level matters. The matters discussed related to other high-density developments in Georgetown, the proposed woonerf concept, parkland dedication, and compatibility of high-density buildings adjacent to low-density residential.

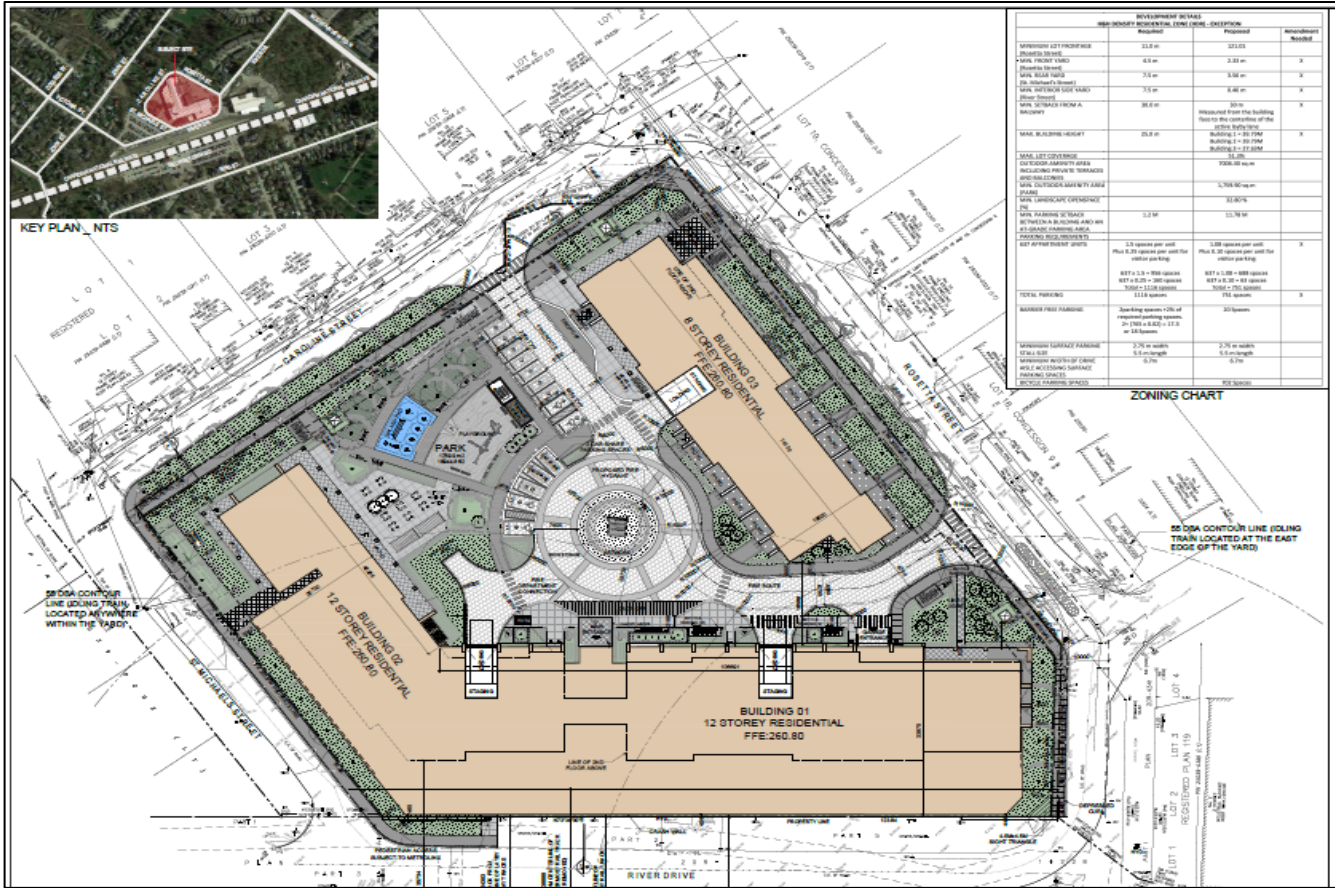
Additionally, a Public Information Meeting has been scheduled for June 22<sup>nd</sup>, 2023, at the Georgetown Public Library to present residents with updated second submission materials. The agent, owners, and pertinent sub-consultants will be in attendance to answer questions.

### 3 Updated Development Proposal

The revised conceptual site plan, prepared by ICON Architects found below resembles the conceptual site plan prepared during the first submission, except for some key changes including:

- Reduction in unit count from 640 units to 637 units, due to a change in unit mix,
- The addition of 44 parking spaces for a total of 751 parking spaces, which allocates 688 spaces for residents and 63 spaces for visitors,
- Increase in bicycle parking spaces from 472 spaces to 702 spaces;
- Inclusion of 2 surface car share spaces over and above the required parking spaces;
- Provision of an additional two barrier-free parking spaces,
- Removal of the sidewalk proposed on the southern boundary within Metrolinx lands as requested by the Town,
- The shifting of the Rosetta Street north to provide a centreline median on Rosetta St. to mitigate potential safety concerns and travel movements,
- Creation of a pedestrian connection for proposed residents from the podium parking area onto the Metrolinx lands for easier pedestrian travel to the Georgetown GO Station. This is accomplished by proposing an access door through the west side of the crash wall and 2.1 metre sidewalk along St. Michael's Street. The approval of the pedestrian connection is subject to the approval of Metrolinx,
- Provision of a 2.1-metre sidewalk and road improvements as well as the identification of pedestrian path of travel throughout the site, including crosswalks at Rosetta Street and Caroline Street,
- The redesign of the ground floor units of Building 3 facing Caroline and Rosetta Streets to resemble maisonettes or townhouse façade with entrances onto the public sidewalk,
- Extension of the green wall system to cover a blank wall enclosing the interior parking area of Building 1,
- Proposal for a green wall on the south façade to provide a visual separation between Buildings 1 and 2,
- Increase in the amount of privately operated public space located on Caroline Street from 1,489 square metres to 1,759 square metres, and,
- Further design consideration of roof-top terraces on all three buildings.

The conceptual site plan, including the development dimensions, is found in **Figure 4-1** while **Figures 4-2 – 4-5** provide renderings of the proposed development, highlighting the built form. Please refer to the Architectural Set, prepared by ICON Architects for more details (i.e., floor plans, building sections).



**Figure 3-1:** Proposed Development Concept Plan, Prepared by ICON Architects



**Figure 3-2:** View of Proposed Development along St. Michael's Street, Prepared by ICON Architects



Figure 3-3: View of River Drive and Rosetta Street, Prepared by ICON Architects



Figure 3-4: View of the Corner of Caroline Street and St. Michaels Street, Prepared by ICON Architects





**Figure 3-5:** View of Rosetta Street Entrance, Prepared by ICON Architects

**Table 3-1** below displays the updated site statistics for the proposed apartment units.

ITEM		PROPOSED
Gross Lot Area		14,355.5 square metres
Minimum Lot Frontage		121.0 metres along Rosetta Street
Number of Dwelling Units		637 units
Maximum Residential Gross Floor Area		48,393.8 square metres
Floor Space Index		3.19
Minimum Front Yard to Rosetta Street		2.3 metres
Minimum Rear Yard to St. Michaels Street		3.5 metres
Minimum Side Yard	Interior to Southern Lot Line	0.4 metres
	Exterior to Caroline Street	5.6 metres
Maximum Building Height		45.5 metres
Minimum Landscaped Area		33.8%
Maximum Lot Coverage		51.3%

**Table 3-1:** Updated Site Statistics

### 3.1 Phasing Plan

As seen in **Figure 3-6** below, the development is proposed to be constructed in three phases which will include the construction of the underground parking structure as illustrated. The first phase will include the construction of the Rosetta Street entrance onto the site, and eastern building identified as Building 1- a 12-storey building, containing 249 units. The second phase contemplates the construction of the adjoining 12-storey building, containing 237 units and located on the western portion of the subject lands. This will be identified as Building '2'. Though Building 1 and Building '2' appear to be one building, the buildings are separated by a shared lobby and corridor. The proposed exterior materials consist of a green wall for the full height of the shared corridor to provide visual separation of the building facades. The third phase will entail the construction of the Caroline Street entrance onto the site, and Building 3, an 8-storey building, containing 151 units on the northern portion of the subject lands.

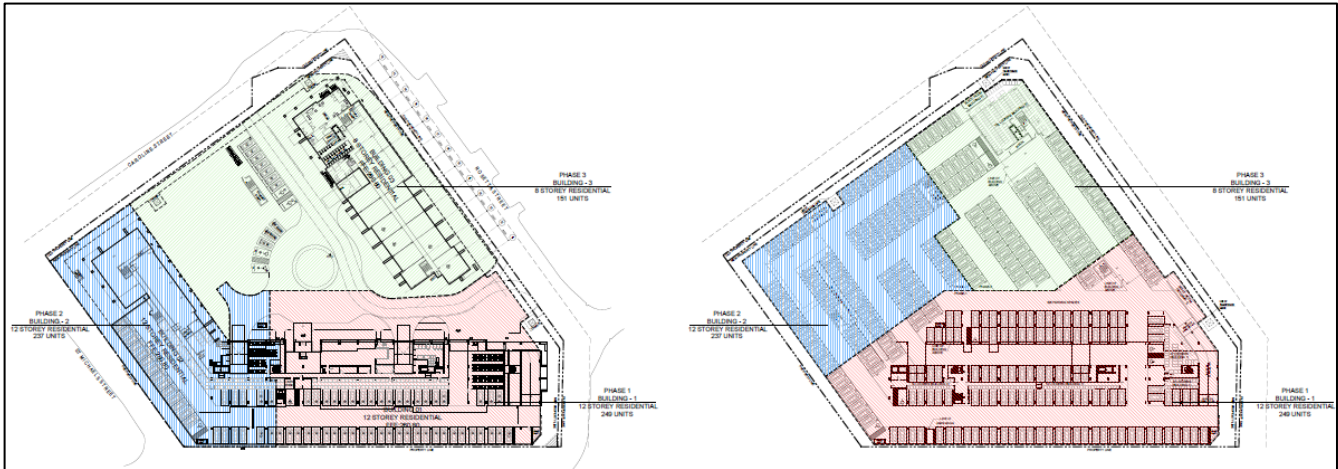


Figure 3-6: Phasing Plan, prepared by ICON Architects

### 3.2 Unit Mix

The proposed unit mix is similar to the first iteration, as it continues to provide a range of 1-to-3-bedroom units planned to accommodate a wide demographic of household types. The proposed unit composition for the residential towers is detailed in the chart below:

UNIT TYPOLOGY	BUILDING 1 12-STOREY	BUILDING 2 12 STOREY	BUILDING 3 8 STOREY	TOTAL
1 bedroom	48	57	37	142
1 bedroom + den + bathroom	90	86	58	234
1 bedroom + den + 2 bathrooms	46	44	21	111
2 bedrooms	32	14	14	60
2 bedrooms + den	22	24	14	60
3 bedrooms	11	12	7	30
<b>Total</b>	<b>249</b>	<b>237</b>	<b>151</b>	<b>637</b>

Table 3-2: Unit Composition for Each Residential Tower

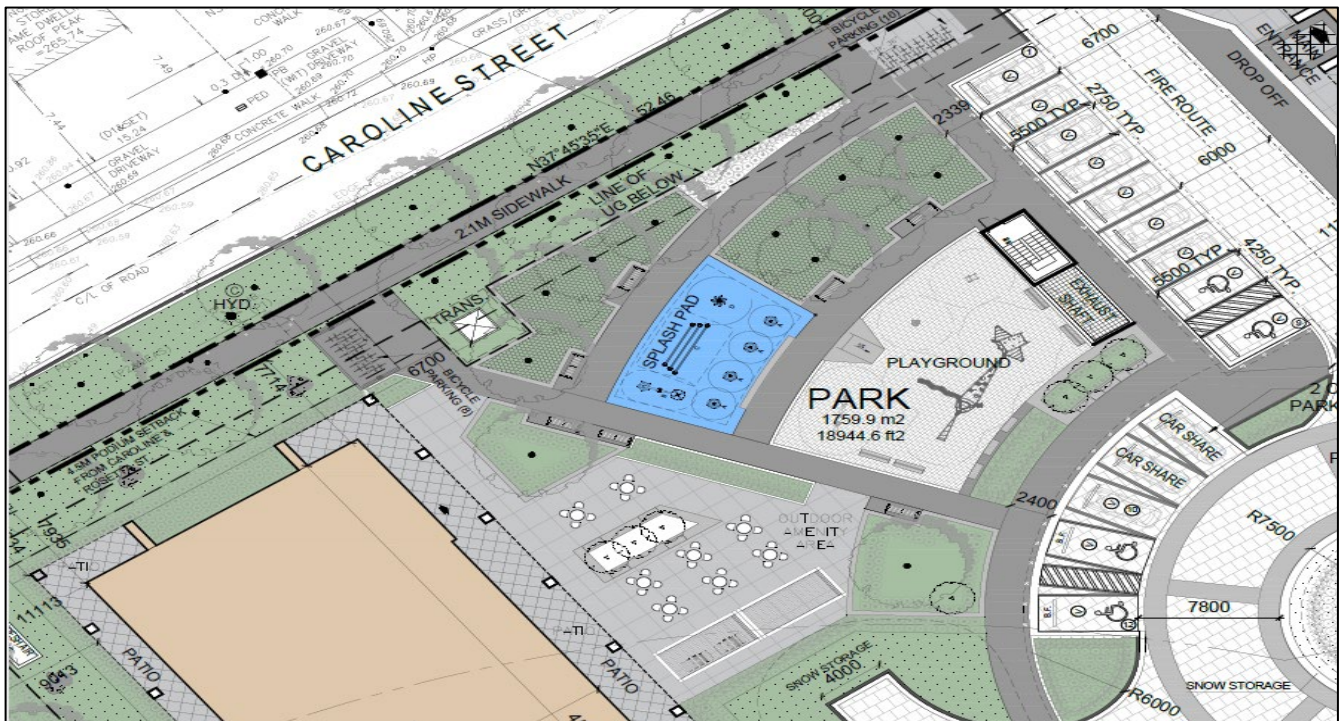
### 3.3 Amenity Spaces

As identified in **Table 3-3**, several indoor amenity spaces are provided for the residents of the proposed development. Indoor amenity spaces in the two-twelve-storey buildings are expected to be shared and accessible by residents of the other. The different programming for the indoor amenity spaces is identified below:

PROGRAM	BUILDING 1 12 -STOREY	BUILDING 2 12 STOREY	BUILDING 3 8 STOREY	TOTAL
Lounge	124.8 square metres	0 square metres	55.3 square metres	180.1 square metres
Party Room	0 square metres	235.8 square metres	147.7 square metres	383.5 square metres
Exercise Room	0 square metres	183.8 square metres	107.3 square metres	291.1 square metres
Kids Play Area	0 square metres	35.6 square metres	31.5 square metres	67.1 square metres
<b>Total</b>	<b>124.8 square metres</b>	<b>455.2 square metres</b>	<b>341.8 square metres</b>	<b>921.8 square metres</b>

**Table 3-3:** Indoor Amenity Area Composition for Each Building

As seen in **Figure 3-7**, approximately 1,759 square metres of privately owned publicly accessible space will be located on the northern portion of the site with frontage onto Caroline Street. This park will be accessible to the community and will operate as a privately owned publicly accessible space (“POPs”), which means that the open space will be privately owned and operated but will be able for the public and surrounding area to use, essentially creating a neighbourhood park for future residents and the larger community. A Park Agreement between the developer and by extension the future condo corporation, and the Town of Halton Hills will be required to ensure that the POPs will remain accessible to the neighbourhood.



**Figure 3-7:** Proposed POP Space, Prepared by ICON Architects

In addition to the POPs, approximately 1,711 square metres of outdoor roof-top terrace space which will be accessible for all for residents are proposed within all three buildings. These roof-top terraces will be subject to the

future site plan approval application however may include a pool, lounge areas, community gardens for seasonal vegetables, barbeque areas, and dining areas. As noted, further detailed design of these amenity spaces will be conducted at Site Plan Approval Stage.

### 3.4 Parking

The vehicular parking component of the proposed development has been increased for a total of 751 parking spaces. **Table 3-4** summarizes the composition of vehicular parking spaces. The two at grade car share spaces are not included in the proposed parking calculation and will be discussed later in this addendum.

LEVEL OF PARKING	VISITOR	RESIDENTS	CAR SHARE	TOTAL
At Grade	63 spaces	20 spaces	2 spaces	85 spaces
Underground Level 1	0 spaces	328 spaces	0 spaces	328 spaces
Underground Level 2	0 spaces	340 spaces	0 spaces	340 spaces
<b>Total</b>	<b>63 spaces</b>	<b>688 spaces</b>	<b>2 spaces (not included within the total count)</b>	<b>751 spaces</b>

**Table 3-4:** Composition of Vehicular Parking Spaces

The number of bicycle parking spaces have increased to 702 bicycle parking spaces. **Table 3-5** summarizes the composition of bicycle parking spaces.

LEVEL OF PARKING	SHORT TERM	LONG TERM	TOTAL
At Grade	64 spaces	306 spaces	370 spaces
Underground Level 1	0 spaces	0 spaces	0 spaces
Underground Level 2	0 spaces	332 spaces	332 spaces
<b>Total</b>	<b>64 spaces</b>	<b>638 spaces</b>	<b>702 spaces</b>

**Table 3-5:** Composition of Bicycle Parking Spaces

### 3.5 Pedestrian Connectivity

Through this iteration, pedestrian connectivity and barrier free accessibility has vastly improved throughout the site and to the surrounding context, through the integration of 2.1-metre public sidewalks surrounding the subject lands, private walkways throughout the site on the proposed ‘woonerf’ and a more direct pedestrian and cyclist connection to Georgetown GO Station, through an opening in the crash wall to access River Street, as seen in **Figure 3-8**. The access through the crash wall is a request from Town of Halton Hills Staff, but needs to be approved by Metrolinx.

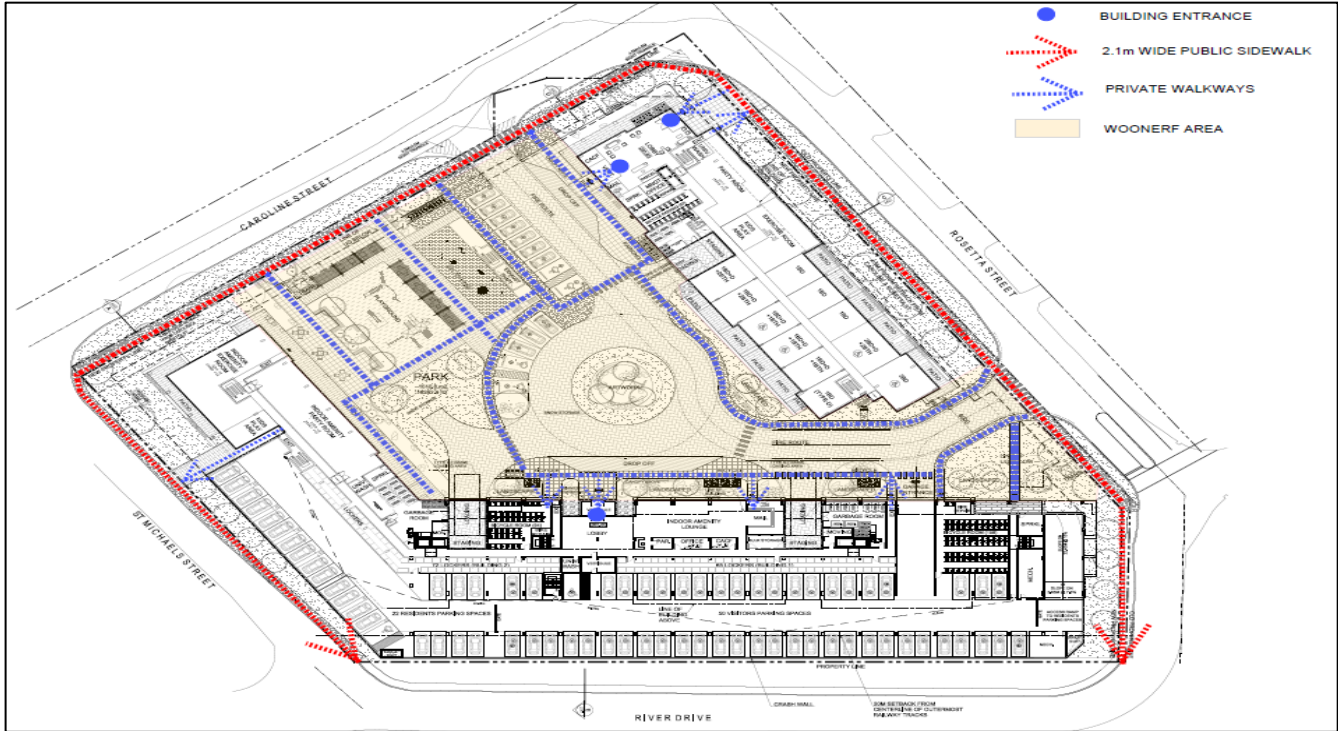


Figure 3-8: Pedestrian Connectivity Map, Prepared by ICON Architects

## 4 Planning Applications

As previously discussed, to permit the proposed development, the following planning applications are required: OPA, and ZBLA, in addition to the future submission of a Draft Plan of Condominium and Site Plan Approval. The following subsections explain the relevance of the OPA and ZBLA.

### 4.1 Official Plan Amendment

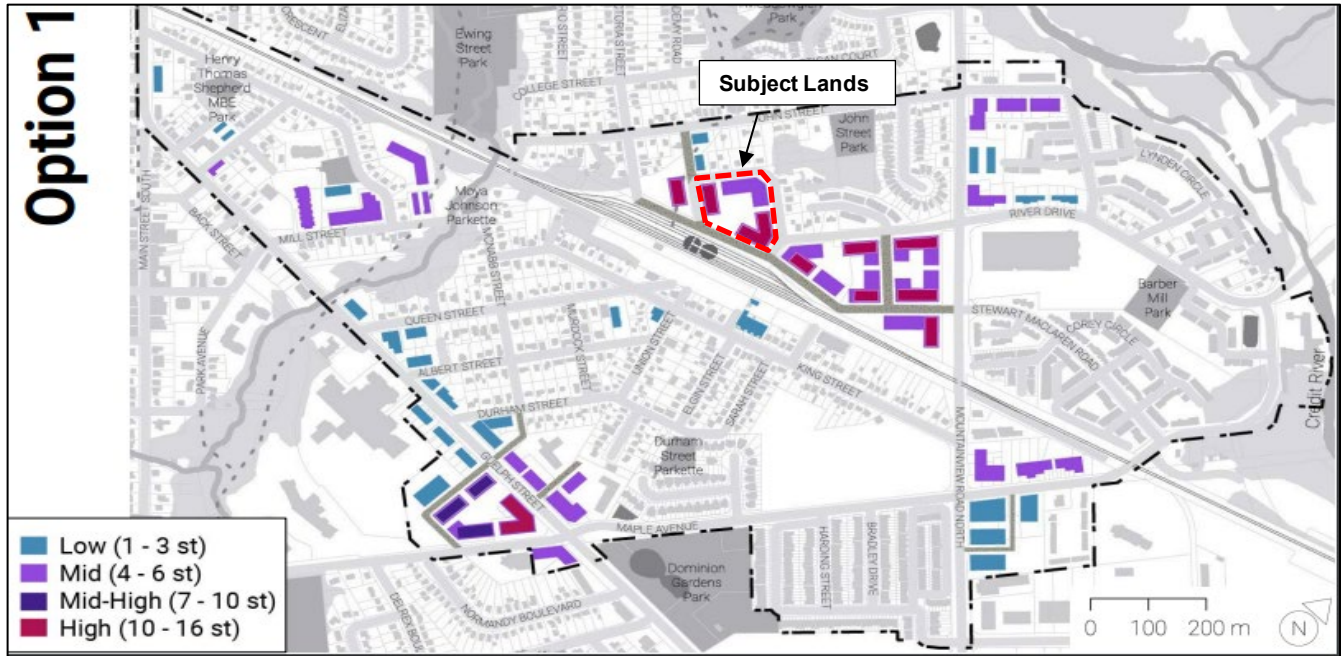
The Town of Halton Hills Official Plan was adopted by Town Council in September 2006 and was approved by Halton Region in March 2008. Schedule A3 of the Official Plan identifies that the subject lands are located within an area that is subject to the Georgetown GO Station Area Secondary Plan.

Schedule H3 within the Georgetown GO Station Area Secondary Plan designates the subject lands as 'High-Density Residential/ Mixed Use Area 2' with a Redevelopment Site Overlay. The permitted uses include apartment dwellings and mixed-use buildings, including high-density residential, office, and a full range of commercial uses. With respect to density and height, a minimum Floor Space Index ("FSI") of 1.8 and a maximum FSI of 2.0 with a maximum height of eight storeys is permitted. The maximum density and height may be further increased through the development process to a maximum FSI of 2.5 for sites within this designation, and a height of 12 storeys for some portions of sites within this designation, subject to bonusing and provided that the Town is satisfied that there will be no significant impact on the surrounding area, based on the submission of detailed plans and models, and a traffic analysis addressing the transportation issues.

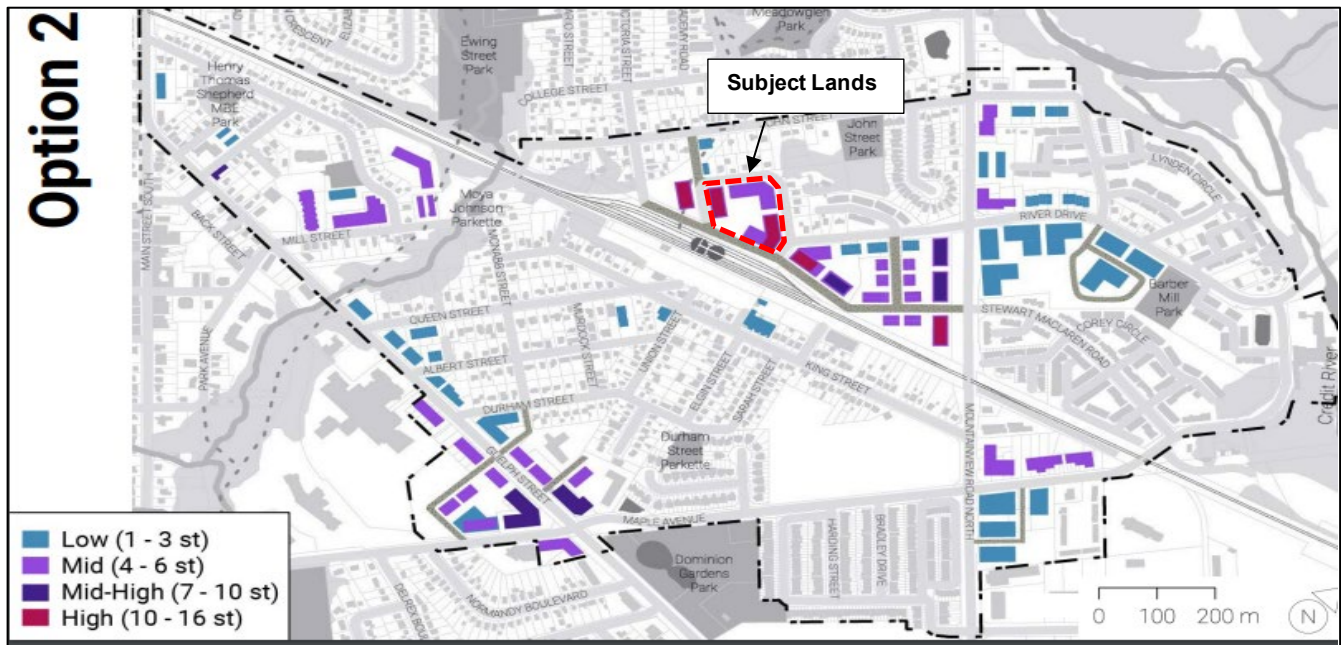
To permit the residential development, an OPA will be required to add a Special Policy Area within the Georgetown GO Station Area Secondary Plan to add a site-specific exception to permit eight storeys for building fronting on local roads (Caroline and Rosetta Street) and an increased height of 12 storeys for buildings fronting onto railway lands (River Drive and St. Michael's Street). In addition, the OPA will provide a site-specific exception to permit an increased density of 3.16 FSI and adherence to the proposed Urban Design Guidelines for the subject lands.

Though a height above eight storeys is requested, the subject lands are located within a Major Transit Station Area, identified as a Strategic Growth Area, planned for higher density uses and intensification. Currently, the GO Station Area/ Mill Street Corridor Secondary Plan is under review. The key objective of the review is to balance

redevelopment and intensification while preserving existing low-density neighbourhoods. As seen in **Figures 4-1 & 4-2**, the options presented during the presentation all contemplate up to sixteen storeys along St. Michaels Street and River Drive. This approach follows the proposed development scheme and corresponding OPA, which proposes higher heights fronting onto River Drive and St. Michael’s Street. Therefore, the consideration of a height above twelve storeys is appropriate, given the proximity to the Transit Station and location within an Intensification Area.



**Figure 4-1:** Option 1 Presented during Georgetown GO Station Area/ Mill Street Corridor Presentation on February 27th, 2023

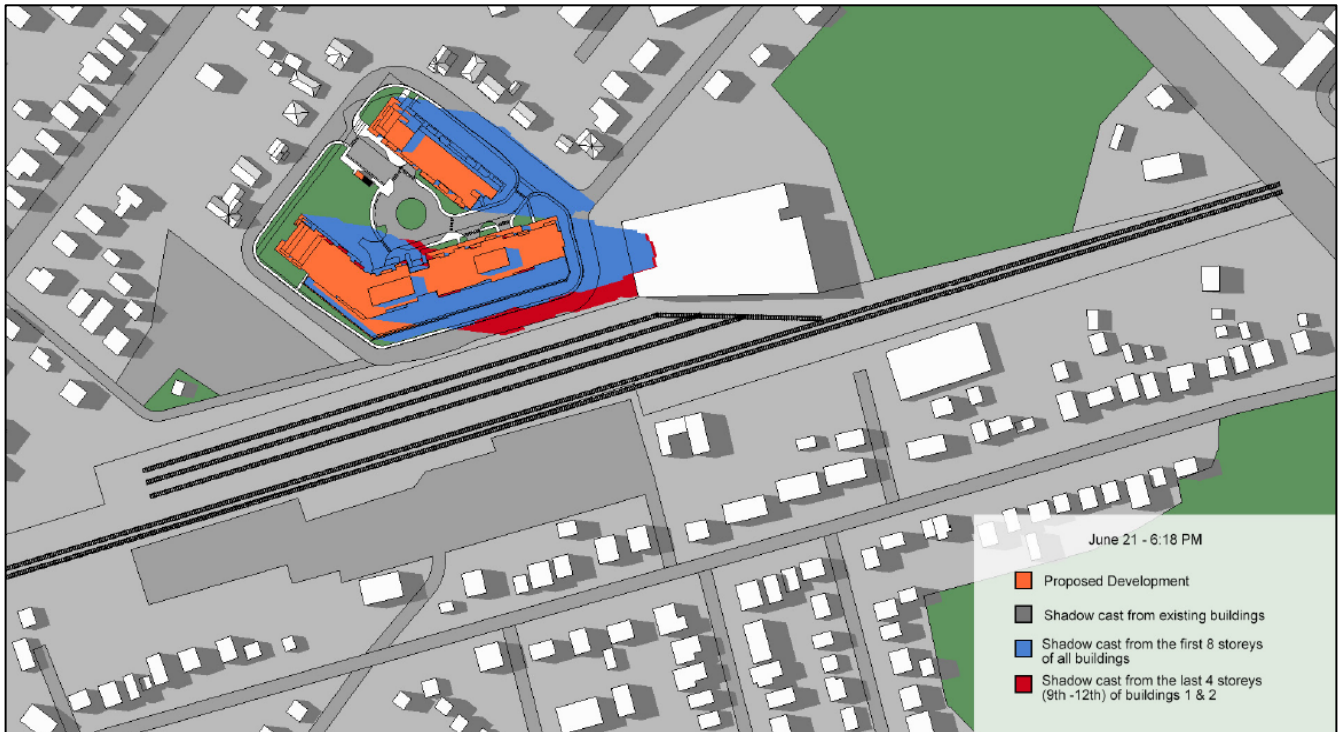


**Figure 4-2:** Option 2 Presented during Georgetown GO Station Area/ Mill Street Corridor Presentation on February 27th, 2023

In terms of compatibility, an increase from 8 to 12 storeys proposed for Buildings 1 and 2 in height has been strategically located to provide the least amount of impact the neighbouring low-density residential. Designated as an intensification area, the proposed interface could be considered as an interim condition as the area continues to

see redevelopment and intensification. The two 12-storey buildings will be located in the southern portion of the subject lands, where it will be opposite an existing industrial building situated at the corner of River Drive and Rosetta Street, the rail line and the Georgetown GO Station parking lot.

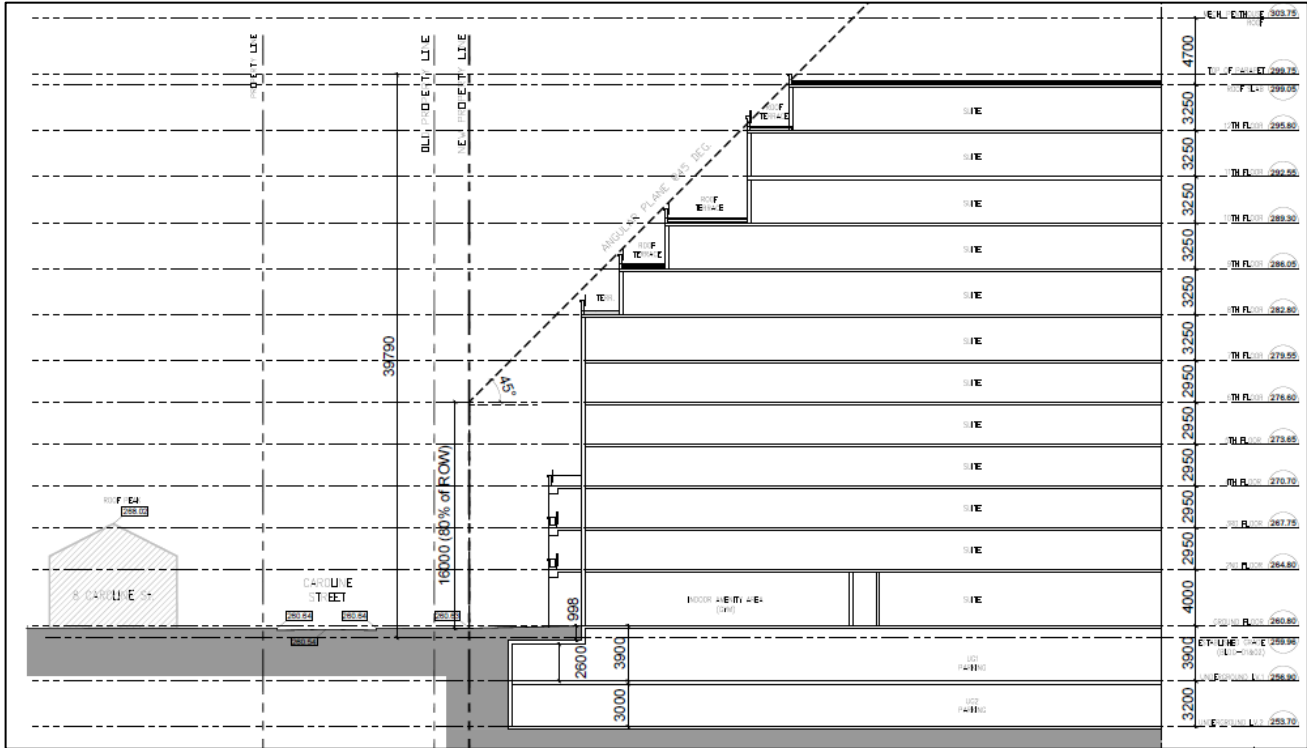
As identified in the accompanying Shadow Study, the site organization will produce narrower shadows that will move quickly across neighbouring properties and the public realm. The Shadow Studies show that there is a minimal difference in shadowing from the increase in four storeys, as it shows the impact difference between the first 8 storeys of all buildings and the last four storeys of buildings 1 and 2, as seen in **Figure 4-3**.



**Figure 4-3:** Sun/Shadow Study, prepared by ICON Architects

Though the proposed options contemplate a mid-rise building of up to 4-6 storeys at the corner of Rosetta Street and Caroline Street, the existing permissions allow for a maximum height of eight storeys. The proposed eight-storey development will ensure compatibility with the surrounding low-density neighbourhood, through the integration of a 45-degree angular plane towards the single detached dwellings, as seen in **Figure 4-3**. The requirement of the 45-degree angular plane applied at the exterior property line at a height equal to 80% of the width of the adjacent right-of-way is established in the accompanying Urban Design Guidelines, which will be required to be adhered to through the OPA.

The 45-degree angular plane provides an appropriate height transition to low-density residential dwellings, by stepping back at three, seven and eight storeys. This establishes a pedestrian-scaled streetscape, while reducing the sense of overlook. In addition, the exterior of the ground floor units is designed to mimic townhouse buildings, providing access and a small yard onto the street, further mitigating the impact of the height along the streetscape. The Caroline Street interface is not anticipated to have any impact as the proposed 'POPs' park will interface with this street and provide visual relief to the height. The inclusion of boulevard street trees on the boundary roads will also assist in softening the streetscape.



**Figure 4-4:** Cross -Section of Caroline Street and the Proposed Development, Prepared by ICON Architects

Please refer to the Draft Official Plan Amendment in **Appendix A**.

## 4.2 Zoning By-law Amendment

The Council for the Corporation of the Town of Halton Hills passed By-law No. 2010-0050 on July 19, 2010. The subject lands are zoned 'Development' (D) within the Town of Halton Hills Zoning By-law, which only permits legally existing uses as of the effective date of the By-law. As such, residential uses are not permitted within the existing Zone.

Therefore, a ZBLA is required to amend Schedule A3-1 within the Town of Halton Hills Zoning By-law to the 'High-Density Residential' (HDR) to allow for the building typology and permitted use of residential units. With reference to the HDR Zone provisions, the proposed ZBLA will also address site-specific provisions relating to the maximum number of dwelling units, maximum height, minimum front yard, minimum rear yard and minimum interior and exterior side yards. The proposed ZBLA will also provide reductions to the maximum number of buildings on the same lot, encroachments into yards, the width of a parking aisle, standard parking rates and special railway setbacks. In addition, in accordance with sanitary servicing supply limits, the proposed ZBLA will add a Holding Zone on the subject lands so that development may commence once the subject lands have received necessary servicing allocations.

Please refer to the Draft Zoning By-law Amendment in **Appendix B** and the proposed modifications, which are written in red in the table below:

REGULATIONS (FOR APARTMENT DWELLINGS)	HDR ZONE	PROPOSED MODIFICATIONS
Minimum Lot Frontage	11.0 metres	121.01 metres (Rosetta Street)
Maximum Number of Dwelling Units	100 units per hectare of lot area	637 units



Minimum Required Front Yard (Rosetta Street)	4.5 metres	2.3 metres
Minimum Required Rear Yard (St. Michaels Street)	7.5 metres	3.5 metres
Minimum Required Interior Side Yard (Southerly Lot Line)	7.5 metres	0.4 metres
The minimum setback from an interior side or rear lot line for buildings that have a height of greater than 13.4 metres is set out	<ul style="list-style-type: none"> <li>a) For the portion of the building that has a building height of 13.4 metres or greater above grade – 13.4 metres</li> <li>b) For that portion of the building that has a building height of 4.6 metres or less above grade – 6.0 metres</li> <li>c) For that portion of the building that has a building height of between 4.6 metres and 13.4 metres above grade – a line that extends upwards at a 45 degree angle way from the lot line</li> </ul>	Does not apply
Minimum Required Exterior Side Yard	6.0 metres	5.6 metres
Maximum Height	25.0 metres	45.5 metres

**Table 4-1:** High Density Residential Modifications

REGULATIONS	GENERAL PROVISIONS	PROPOSED
Exceptions to Height Requirements	Any mechanical features, such as structures containing a mechanical penthouse or the equipment necessary to control an elevator provided that such equipment does not project more than 5 metres above the highest point of the roof and does not occupy greater than 50% of the area of the roof	Any mechanical features, such as structures containing a mechanical penthouse or the equipment necessary to control an elevator provided that such equipment does not project more than 5.5 metres above the highest point of the roof

Special Railway Right of Way Setbacks	All buildings and structures containing a dwelling unit shall be located no closer than 30 metres from the right-of-way owned by a Federally regulated railway company.	Shall not apply
Minimum Surface Parking Stall Size	2.75 metres x 5.5 metres length	2.75 metres width x 5.5 metres
Minimum Underground Parking Stall Size	2.6 metres x 5.5 metres length	2.6 metres x 5.5 metres length
Minimum Width of Drive Aisle Providing Access to a Parking Space within a Parking Area	6.7 metres	6.0 metres
Minimum Parking Setback between a Building and an at-grade parking area	1.2 metres	11.7 metres
Parking Requirements of Apartment Units	1.5 spaces per + 0.25 spaces per unit for visitor parking	1.08 spaces per unit + 0.10 spaces per unit for visitor parking
Maximum Number of Buildings on the Same Lot	1	3

Table 4-2: General Provisions Modifications

#### 4.2.1 Minimum Required Front Yard

The reduction to the minimum front yard provision is in relation to Building 1. Due to the shape of the road pattern around the subject lands, that portion of the Building 1 will be located closer to the street than other buildings.

#### 4.2.2 Minimum Required Rear Yard

The intent of a rear yard setback is to allow for the provision of suitable private amenity spaces and maintain compatibility with abutting uses. All the residential units will have their private amenity space. The abutting uses are St. Michaels Street, which is expected to be closed for vehicular traffic and a Metrolinx parking lot. Therefore, there are no compatibility concerns relating to uses.

#### 4.2.3 Minimum Required Interior Yard

Considering Rosetta Street is determined to be the front lot line, the interior side yard is considered along River Drive, which is owned by Metrolinx. The intent of a side yard setback is to maintain compatibility between surrounding uses and setbacks from neighbourhood. There are no concerns regarding setbacks from River Drive, as this is owned by Metrolinx and currently used as an access route to the GO Station Parking Lot.

#### 4.2.4 Minimum Required Exterior Side Yard

The reduction to this provision is minor and is in relation to the eight-storey portion of Building 2 at the corner of St. Michael's Street and Caroline Street. The proposed patio space is located approximately 5.6 metres from the property line, while the building face is approximately 7.9 metres from the property line.

#### 4.2.5 Maximum Height

The proposed ZBLA is requesting to increase the maximum height to twelve storeys (45.5 metres), in conjunction with the proposed OPA to the Town of Halton Hills Official Plan. The intent of the maximum height provision is to restrict the built form.

Though a height above eight storeys is requested, the subject lands are located within a Major Transit Station Area, identified as a Strategic Growth Area, planned for higher density uses and intensification. Currently, the GO Station Area/ Mill Street Corridor Secondary Plan is under review. As seen in **Figures 4-1 & 4-2**, the options presented during the presentation all contemplate up to sixteen storeys along St. Michaels Street and River Drive. This approach follows the proposed development scheme, which proposes higher heights fronting onto River Drive and St. Michael's Street. Therefore, the consideration of a height above twelve storeys is appropriate, given the proximity to the Transit Station and location within an Intensification Area.

In terms of compatibility, an increase from 8 to 12 storeys in height will not significantly negatively impact the neighbouring low-density residential. The two 12-storey buildings will be located in the southern portion of the subject lands, where it will be opposite an existing industrial building situated at the corner of River Drive and Rosetta Street, the rail-line and the Georgetown GO Station parking lot. The Shadow Studies show that there is a minimal difference in shadowing from the increase in four storeys, as it shows the impact difference between the first 8 storeys of all buildings and the last four storeys of buildings 1 and 2, as seen in **Figure 4-3**.

#### **4.2.6 Exception to Height Requirement**

This modification is a minor adjustment from the required height of mechanical penthouses. There is no impacts relating to shadowing, and privacy as a result of this modification.

#### **4.2.7 Special Railway Right of Way Setbacks**

River Drive is owned by Metrolinx; therefore the special rail right-of-way setbacks should not apply. The intent of this provision is to minimize noise, vibration and safety issues for developments located in proximity to the railway. The proposed development is located no closer than 30 metres from the building face to the centerline of the active lay-by line. Preliminary correspondence from Metrolinx has indicated that the setback is considered appropriate and will be confirmed through future submissions, as seen in **Appendix C**. Comments from Metrolinx review consultant, AECOM is in review of the proposed crash wall design, noise and vibration technical studies which have been updated and included in this submission.

#### **4.2.8 Parking Requirement**

As confirmed by the TIS prepared by Paradigm Transportation Consulting Ltd, the location of the subject lands and proximity to the GO Station are positive attributes that help justify a reduced parking supply. Please refer to the TIS for more details.

#### **4.2.9 Minimum Drive Aisle Width**

The reduction to this provision is minor and is a result of the configuration of the underground parking aisle width. There are no concerns as a relation of the reduction, as there is sufficient space for cars to access parking spaces,

## **5 Revised Supporting Technical Reports and Plan**

Various studies, reports, and material were prepared in support of the original development application, including:

- Environmental Site Assessment
- Urban Design Guidelines
- Sun/Shadow Study
- Heritage Impact Assessment
- Functional Servicing Report & Stormwater Management Report
- Geotechnical Report
- Noise/Vibration Study
- Air Quality Study

- Transportation Impact Study
- Tree Inventory/ Preservation Plan.

As previously discussed, Arcadis received comments for the Town and external agencies in Fall/Winter of 2022. Based on comments received from the applicable agencies and taking into consideration the changes proposed in the revised conceptual plan, the following supporting studies and reports have been revised to address the comment received:

- Updated Urban Design Guidelines with minor grammatical/ punctuation corrections, dated May 2023
- Sun/Shadow Study, dated May 2023
- Revised Functional Servicing and Stormwater Management Report, dated May 2023
- Updated Noise/Vibration Study, dated May 2023
- Revised Air Quality Study, dated April 2023
- Revised Transportation Impact Study, dated May 2023
- Tree Inventory/Preservation Plan
- Landscape Plan

## 5.1 Sun/Shadow Study

A Sun/Shadow Study was prepared by ICON Architects in February 2022. A revised Sun/Shadow Study was prepared by ICON Architect in May 2023. The main conclusion from the study was that shadow impacts caused by the proposed development are acceptable, as minimal impacts can be seen on neighbouring streets and shadow sensitive properties and there are no impacts on parks, open space or natural areas. In addition, there is minimal change in shadowing from increase from 8 to 12 storeys.

## 5.2 Functional Servicing Report and Stormwater Management Report

A preliminary Functional Servicing Report (“FSR”) was prepared by Arcadis in May 2022. A revised FSR was prepared by Arcadis in June 2023 to address comments and the updated unit count. Key conclusions from the report include:

- Storm Sewer and Stormwater Management: Storm flows shall be attenuated on-site and released to the municipal storm sewer at an appropriate discharge rate thus meeting the City’s target for quantity control. By incorporating inherently clean rooftop, landscape, and a media filtration unit, the site will meet the City’s target for quality control.
- Sanitary Sewers: As the site represents a manageable increase in sanitary flow, the proposed development can proceed without improvements to the municipal sewer system.
- Water Supply: The existing municipal water supply has sufficient capacity to support the proposed fire and domestic water demands without improvements to the system.

## 5.3 Environmental Noise and Vibration Study

An Environmental Noise and Vibration Study was prepared by SLR in March 2022. An updated Environmental Noise and Vibration Study was prepared by SLR in May 2023 to address comments and the updated Site Plan. Key conclusions and recommendations from the study include:

- An assessment of transportation noise impacts has been completed. Based on transportation façade sound levels upgraded glazing is required within the development.
- Ventilation requirements include a combination of Mandatory AC and Provision for Future Installation of AC, as outlined in Section 2.6 and Appendix D. Warning Clauses requirements include those for CN and Metrolinx, for all units.

- Warning Clauses should be included in agreements registered on Title for the residential units and included in agreements of purchase and sale/rental agreements and include a combination of MECP Type C and Type D warning clauses. In addition, the CN and Metrolinx warning clauses are recommended for all units.
- Rail vibration levels were measured at the existing site in the approximate area of the building footprint location and at a location closer to the rail right-of-way. The maximum vibration levels were found to meet the CN/GO criteria. No mitigation is required.
- Expected vibration levels from potential future rail traffic along closer tracks is not expected to be of concern, based on assessment of vibration propagation from existing rail traffic.
- Stationary noise with the potential to impact the development includes the Georgetown GO Train Layover Yard to the south. It is recommended that the site be designated as Class 4 by the land-use planning authority, due to the predicted impacts of the Georgetown GO Train Layover Yard on the proposed residential development.
- In addition to Class 4 designation, enclosed noise buffers (ENBs) are required along a portion of the south and east facades of Building 01 and west façade of Building 02, where residential units are planned.
- Warning Clauses should be included in agreements registered on Title for the residential units and included in agreements of purchase and sale/rental agreements. MECP Type E and Type F warning clauses are required for all units.
- Mandatory AC is required for all units within the development as a component of the Class 4 Designation.
- The proposed Heritage Road Layover Yard is scheduled for construction with completion expected in 2026/2027. SLR understands the Georgetown Layover Yard is reaching the end of its serviceable life, and it will be replaced with the proposed Heritage Road Layover Yard. Once the Heritage Road Layover Yard is fully operational, the Georgetown GO Layover Yard is not expected to be a stationary source with the potential to impact the Project, and the above noted noise controls (ENBs, Type F warning clause, and mandatory AC) will no longer be required.

## 5.4 Air Quality Study

An Air Quality Study, Dust and Odour Compatibility and Migration Study was prepared by SLR in March 2022. A revised Study was prepared by SLR in April 2022 to address comments and the updated Site Plan. The main conclusion was that adverse air quality emissions from surrounding industries are not anticipated at the proposed development, and that the proposal is not anticipated to limit the operations of surrounding industries.

The following mitigation measures are recommended:

- Placement of ventilation intakes in rooftop mechanical spaces, aboveground above-grade locations, or on the opposite side of the rail corridor (i.e., on the north façade) to provide separation distance from railway emissions and include MERV rated filters on fresh air intakes;
- Positive pressurization following ASHRAE standards to help reduce the potential for outside air to passively enter the building; and
- Enclosed Noise Buffer Balconies (ENBB's), as per current Provincial guidance are recommended for noise mitigation. The ENBB should also help to reduce the potential for future complaints from potential odour emissions

The recommended mitigation measures are intended to minimize the potential for future complaints from diesel emissions and associated odours at the subject lands resulting from idling locomotives in the nearby layover yard. It is expected that the Georgetown GO Layover Yard will move to the Heritage Road Layover Yard, therefore these controls will no longer be required. A reassessment of air quality control measures should be completed once the Heritage Road Layover is confirmed to proceed and the anticipated schedule for completion is available.

## 5.5 Transportation Impact Study

A Transportation Impact Study ("TIS") was prepared by Paradigm Transportation Solutions Ltd. in April 2022. A revised TIS was prepared by Paradigm in May 2023 to address comments and the updated unit count. Key conclusions from the study include:

- Base Year Traffic Operations: No critical movements are noted in the base year operations at the study area intersections.
- Estimated Site Generated Traffic: The subject lands are estimated to generate approximately 187 vehicle trips during the AM Peak Hour and 215 vehicle trips during the PM Peak Hour.
- Background Traffic Operations – Five Year Horizon: As the traffic volume increases, capacity issues (v/c ratio greater than 0.85) are forecast for the westbound approach at the Mountainview Road North and River Drive during the PM Peak Hour.
- Background Traffic Operations – Ten Year Horizon: As the traffic volumes increase, the capacity issues outlined under the five-year horizon are expected to continue to occur.
- Total Traffic Operations – Five Year Horizon: The capacity deficiencies identified under background conditions will deteriorate with the addition of site-generated traffic. No new capacity issues are triggered by the addition of site generated traffic. No capacity issues are forecast to occur at any other study area intersection or the site driveways.
- Total Traffic Operations – Ten Year Horizon: The capacity deficiencies identified under background conditions will deteriorate with the addition of site generated traffic, The addition of site-generated traffic trigger capacity issues for the shared northbound left/through movements at the Mountainview Road North and River Drive intersection. No capacity issues are forecast to occur at any other study area intersections.
- St. Michaels Street Extension: The extension of St. Michael’s Street to John Street will have negligible impact on traffic operations throughout the study area and is not required from a capacity perspective. The unopened right-of-way would be better used as an Active Transportation (AT) connection between John Street and Caroline Street. Desire lines through the grassed area were observed in the field suggesting the need for this AT connection.
- Remedial Measures: To manage the expected growth in traffic at the Mountainview Road North and River Drive intersection, the road authority should consider:
  - Optimize signal timings with a protected westbound left-turn phase;
  - Reduce the southbound approach to one shared through/right lane and one left turn lane with 50 metres of storage
  - Provide a northbound left turn lane with 50 metres of storage
  - The reconfiguration of the northbound and southbound lane grouping appear to be feasible without modifications to the existing pavement width. The revised land configuration can likely be achieved by modifying the existing pavement markings and signage.
  - No new traffic control signals are recommended at the intersection of Mountainview Road North and John Street and no left-turn lanes are recommended at the proposed site driveways.
- Parking Study: The subject land’s proposed parking supply is identified as 751 spaces with an allocation of 688 spaces for occupants and 63 spaces for visitors.
  - The site’s parking supply does not meet the Town’s Zoning By-law requirements. The site’s parking supply is identified as 1.18 spaces per units.
  - Vehicle ownership data from the Transportation Tomorrow Survey (TTS) for apartment units in zones surrounding GO Transit stations along the Kitchener Line between Kitchener and Mount Pleasant indicate an occupant parking demand for the subject site of 304 spaces. Visitor parking is not included in this calculation. Including the visitor parking demand, estimated by the Zoning By-law, 160 visitor parking spaces, the overall parking demand for the site is estimated to be 544 spaces.
  - The ITE Parking Generation Manual indicates a parking demand ranging from 655 to 696 parking spaces (occupant and visitor).
  - Proxy site data from multiple sites with similar neighbourhood characteristics indicates a parking demand of approximately 595 spaces (occupant and visitor). With a parking supply of 751 spaces, the site’s parking demand is forecast to be less than the proposed supply.

- The TDM Checklist from the Region of Waterloo identifies a total of 13% reduction in parking spaces based on the proposed TDM program. This results in a forecast parking demand of 518 spaces.
- Using several different methodologies, the proposed development is estimated to have a parking demand in the order of 518 spaces to 647 spaces. With a parking supply of 751 spaces, the site's parking demand is forecast to be accommodated by the on-site parking.
- Transportation Demand Management: The site's concept plan includes a robust TDM program that can assist in mitigating the site's transportation and parking impacts on the adjacent road network, promote a strong and vibrant economy, and create a livable community that has a balanced transportation network. The monitoring and adjustment of the site's TDM program will be critical to the site's success.
- Key recommendations include:
  - At the Mountainview Road North and River Drive intersection, the road authority should consider:
    - Optimize signal timings with a protected westbound left-turn phase
    - Reduce the southbound approach to one shared through/right lane and one leftturn lane with 50 metres of storage
    - Provide a northbound left turn lane with 50 metre of storage.
    - The revised lane configuration can likely be achieved by modifying the existing pavement markings and signage.

## 6 Current Planning Status

As noted earlier in this report, the original PJR provided a fulsome assessment of the proposed development. Many planning policy discussions would mirror the planning justification found within the original PJR. Therefore, for the purposes of this addendum report, the detailed planning analysis will focus on addressing comments from the Town.

### 6.1 Town of Halton Hills Official Plan

The Town of Halton Hills Official Plan was adopted by Town Council in September 2006 and was approved by Halton Region in March 2008. The Official Plan sets out the Town's policies for growth and development through to 2031 and applies to all lands within the Town. It is intended to serve as the basis for making land use decisions and managing change. It contains goals, objectives and policies established primarily to manage and direct physical change and the effects of the social, economic and natural environment.

For the purposes of this report, the December 2020 consolidated version of the Official Plan available online at <https://www.haltonhills.ca/en/business/official-plan.aspx> was used.

#### 6.1.1 Part D Community Vision, Goals and Strategic Objectives

##### Section D1 Living Areas

##### Subsection D1.4 General Residential and Housing Policies

##### Policy D1.4.3 Infill Development in Established Residential Neighbourhoods

*Infill development, in accordance with the applicable land use designation in this Plan, shall be encouraged provided Council is satisfied that:*

- a) *the proposed development, including building form and density, is compatible with the character of the existing neighbourhood;*
- b) *new buildings are designed in a manner that is sensitive to the location, massing and height of adjacent buildings*
- c) *a suitable transition in lot sizes, densities, building forms, and heights is provided from an adjacent development*

**Planning Comment:** As indicated in Section 2.2., the surrounding neighbourhood consists of low-density, and employment uses. Several technical studies to address compatibility and impact on adjacent uses were completed

in support of the proposed development, including Traffic Impact Analysis, Cultural Heritage Impact Analysis, Air Quality Study, Sun Shadow Study, and Noise/ Vibration Study.

While recognizing that this area is slated for intensification and higher density uses, the proposed development attempts to strike a balance between providing redevelopment to address intensification within a designated MTSA area and mitigating impacts to the existing low-density neighbourhoods. The site layout will place the higher density buildings on the southern portion, where it will impact the least number of low-density residential properties. As identified in the Shadow Study, the proposed site organization will produce narrower shadows that will move quickly across neighbouring properties and the public realm.

The proposed eight-storey development will ensure compatibility with the surrounding low-density neighbourhood, through the integration of a 45-degree angular plane towards the single detached dwellings, as seen in **Figure 4-3**. The requirement of the 45-degree angular plane applied at the exterior property line at a height equal to 80% of the width of the adjacent right-of-way is established in the accompanying Urban Design Guidelines, which will be required to be adhered through the OPA.

The 45-degree angular plane provides an appropriate height transition to low-density residential dwellings, by stepping back at three, seven and eight storeys. This establishes a pedestrian scaled streetscape, while reducing the sense of overlook. In addition, the exterior of the ground floor units is designed to imitate townhouse buildings, providing an access and a small yard onto the street. The Caroline Street wall is interrupted with the proposed placement of the 'POP' park.

- d) *existing trees and vegetation will be retained and enhanced where possible and additional landscaping will be provided to integrate with the existing neighbourhood;*

**Planning Comment:** The subject lands are an existing industrial building with the majority of the land area paved as a product of the previous manufacturing activities. A total of 10 trees were inventoried within the Tree Inventory Protection Plan, prepared by the Urban Arborist in March 2021. It identified that all the trees will have to be removed due to them being all within the development footprint. A Landscape Plan has been prepared by MEP Design, illustrating a planting plan and the implementation of boulevard street trees to integrate with the surrounding neighbourhood. The redevelopment of the subject lands will provide for street trees and landscaped areas and will contribute to the Town's urban canopy.

- e) *the proposed development will not create a traffic hazard or an unacceptable increase in traffic in local roads*

**Planning Comment:** The TIS prepared by Paradigm concluded that the impact of the forecast site generated vehicle trips will have a minor impact on the adjacent road network.

- f) *significant views and vistas which help define a residential neighbourhood are preserved*

**Planning Comment:** The historical context of the neighbourhood was industrial in nature given the surrounding industrial commercial buildings and the location of the GO Station and the associated layover tracks. The future condition being proposed by Metrolinx will see the removal of the layover facility to an area outside of the urban area. Significant south views and vistas will therefore be improved in the area with the proposed improvements to the GO Station as well as the streetscape improvements as part of the proposed development. Connections and visibility to historic Georgetown GO Station will be maintained, through improved pedestrian connections on St. Michaels, and the series of step backs at upper levels, façade articulation, recessing balconies to preserve a degree of views along Rosetta Street. Further, vistas will be enhanced through the planting of street trees along the boulevard, which will create an inviting pedestrian experience to access the vistas towards the Georgetown GO Station.

## 6.1.2 Part H3 Georgetown GO Station Area Secondary Plan

### Subsection H3.3 General Policies

#### Subsection H3.3.7 Transportation

##### b) Road Network

*The road network for the Secondary Plan area is established on Schedules B1 and B2 of the Official Plan. However, in addition, as part of any redevelopment application in the North Precinct, a traffic analysis shall be*



required including a review of **possible modifications** to the road network. Modifications which shall be considered through such an analysis include:

- i) creation of additional road connections through the area around the GO Station to improve traffic movement and enhance the ability for area residents, GO patrons and GO buses to access the station and/or the proposed new development. Two potential connections are proposed, the extension of St. Michaels Street to Rosetta Street and the extension of St. Michaels Street to John Street as designated on Schedule H3. The extension of St. Michaels Street to Rosetta Street will require the acquisition of lands or an easement from the railway

**Planning Comment:** Several discussions have occurred with Town Staff regarding the extension of St. Michael's Street to John Street and the closing of St. Michaels Street from Caroline Street to River Drive. Transportation Engineering advised that they were of the opinion that St. Michaels Street would need to be closed from Caroline Street to River Drive, while being extended to John Street to accommodate the proposed vehicular traffic at the owner's cost. However, as indicated in the policy above, these are possible modifications and potential connections that can be explored. As seen in **Figure 6-1**, during the Georgetown GO Station Secondary Plan Update Presentation, options shown do not identify St. Michael's Street to be closed in the draft Secondary Plan massing. As such, the closure of River Drive or St. Michaels Street has not been made official. Therefore, we propose that the consideration for St. Michaels Street extension or any cul-de-sac design should be revisited when the decision on these roads have been made. In addition, the TIS concluded that the extension of St. Michael's Street to John Street will have negligible impact on traffic operations throughout the study area and is not required from a capacity perspective. The unopened right-of-way would be better used as an Active Transportation (AT) connection between John Street and Caroline Street. Desire lines through the grassed area were observed in the field suggesting the need for this AT connection. In a servicing perspective, no services are proposed to extend onto John Street. As detailed in the updated FSR, the proposed development is sufficiently serviced through the proposed connections to the existing infrastructure located on Rosetta Street and River Drive.



Figure 6-1: Option 1 Massing Presented during Georgetown GO Station Secondary Plan

### Subsection H3.5.5 High Density Residential Mixed Use Area I

- c) *Bousing*

*in addition to the bonusing triggers in Section G.4.3.1 of the Official Plan, the following additional bonusing triggers are applicable:*

- i) Development which reflects the principles of sustainable development by including three or more significant attributes which encourage energy efficiency, conservation of resources and minimization of environmental impacts (e.g. green or reflective roofs, LEED Certification, water recycling)*
- ii) Provision of parking, direct pedestrian links to the GO Station and other facilities which are shared with the GO Station and provide a service to transit users;*
- iii) Development of a public parkland and/or private open space which is designed to be accessible to the public, as well as residents of the development and;*
- iv) Provision of affordable and special needs housing.*

### **Subsection H3.5.6 High Density Residential/ Mixed Use Area II**

#### *b) Timing, Density and Height*

*The redevelopment of areas within this designation, for any residential development shall be prohibited either until the relocation of the rail layover facility or until studies are submitted that satisfy the Town, in consultation with CN and Metrolinx, that noise and vibration can be appropriately mitigated in conformity with Ministry of Environment guidelines*

*With respect to density and height, a minimum Floor Space Index (FSI) of 1.8 and a maximum FSI of 2.0, with a maximum height of 8 storeys, is permitted. In addition, the maximum density and height may be further increased through the development process to a maximum FSI of 2.5 for sites within this designation, and a height of 12 storeys for some portions of sites within this designation, subject to provisions of Section G4.3 of the Official Plan, and provided that the Town is satisfied that there will be no significant impact on the surrounding area, based on the submission of detailed plans and models, and a traffic analysis addressing the transportation issues identified in Section H3.3.7. In addition to the bonusing triggers in Section G4.3.1 of the Official Plan, the additional triggers in H3.5.5 of this Plan are applicable.*

**Planning Comment:** The proposed OPA will be required to add a site-specific exception to permit eight storeys for building fronting on local roads (Caroline and Rosetta Street) and an increased height of 12 storeys for buildings fronting onto railway lands (River Drive and St. Michael's Street). In addition, the OPA will provide a site-specific exception to permit an increased density of 3.16 FSI and adherence to the proposed Urban Design Guidelines for the subject lands.

Though a height above eight storeys is requested, the subject lands are located within a Major Transit Station Area, identified as a Strategic Growth Area, planned for higher density uses and intensification. Currently, the GO Station Area/ Mill Street Corridor Secondary Plan is under review. As seen in **Figures 4-1 & 4-2**, the options presented during the presentation all contemplate up to sixteen storeys along St. Michaels Street and River Drive. This approach follows the proposed development scheme and corresponding OPA, which proposes higher heights fronting onto River Drive and St. Michael's Street. Therefore, the consideration of a height above twelve storeys is appropriate, given the proximity to the Transit Station and location within an Intensification Area.

In terms of compatibility, an increase from 8 to 12 storeys in height will not significantly negatively impact the neighbouring low-density residential. The two 12-storey buildings will be located in the southern portion of the subject lands, where it will be opposite an existing industrial building situated at the corner of River Drive and Rosetta Street, the rail-line and the Georgetown GO Station parking lot. The location will strategically impact the least number of low-density residential properties and produce narrower shadows that will move quickly across neighbouring properties and the public realm. The proposed development will ensure compatibility with the surrounding low-density neighbourhood, through the integration of a 45-degree angular plane towards the single detached dwellings. The 45-degree angular plane provides an appropriate height transition to low-density residential dwellings, by stepping back at three, seven and eight storeys. This establishes a pedestrian scaled streetscape, while reducing the sense of overlook. In addition, the exterior of the ground floor units is designed to mimic townhouse buildings, providing access and a small yard onto the street. The Caroline Street wall is broken with the proposed placement of the 'POP' park.

In terms of bonusing, the More Homes, More Choice Act passed in 2019 completely repealed Section 37 formerly permitting by-laws for density and height bonusing. The Section was replaced with the new Community Benefit By-

law regime that can be imposed by municipalities to recover costs for parks, other recreational lands, services ineligible for DC recovery or could be included in the DC by-law. However, it is our opinion that the passing of the CBC by-law effective September 1, 2022 does not extinguish the OP policies implementing the bonusing structure when considering an application, only the ability to enter into a bonusing agreement under Section 37 of the Planning Act RSO 1990.

Therefore, Section G.4.3.1 of the Official Plan provides a list of what the Town would consider a significant public benefit. If conditions are met for bonusing under this section, the Official Plan permits Council to pass a site-specific by-law establishing the detailed development standards and may require the applicant to enter into a bonusing agreement. These bonusing triggers include:

- Provision of long term care or retirement homes
- Needed community facilities
- Provision of Parkland over and above what is required
- Provision of transit facilities
- Conservation/enhancement of a significant built heritage resource
- Provision of affordable or special needs housing
- Provision of public art in a key location

As seen above, Section H3.5.6 provides additional bonusing triggers applicable to proposals located within the Georgetown GO Station Secondary Plan. In our opinion, the proposed development will provide several benefits for existing and future residents, including a direct pedestrian link to the GO Station, through an opening in the crash wall for residents to access River Street. This promotes sustainable active transportation options for residents and commuters alike accessing the Georgetown GO Station and VIA Rail, which provides service along the Kitchener Line, providing connections to Brampton, Mississauga and Toronto.

Discussions with the Town are ongoing on what aspects of the proposed development should be considered bonusing or parkland dedication. Nevertheless, the proposed development is providing approximately 1,759 square metres of privately owned publicly accessible space will be located between Buildings 2 and 3. This park will be accessible to the community and will operate as a privately owned publicly accessible space (“POPs”), which means that the open space will be privately owned and operated but will be able for the public and surrounding area to use. It will not be fenced in and located with frontage onto Caroline Street, creating a neighbourhood park for the surrounding area to utilize.

The proposed development will also provide additional benefits, including the conservation of a significant built heritage feature through the implementation of the CHIA recommendations to preserve and display salvaged parts of the building as art and to provide an information plaque.

Proposals for bonusing under these OP provisions will require the approval of Council.

## 7 Conclusions and Recommendations

1 Rosetta Street Inc. (“the owner”) has interest in approximately 1.34 hectares of land, municipally referred to as 1 Rosetta Street and 6 & 8 St. Michael’s Street in the Town of Halton Hills (herein referred to as the “subject lands”). The owner is proposing to redevelop the subject lands into a residential development, consisting of one twelve storey, 249-unit building, one twelve-storey, 237-unit building and one eight-storey, 151-unit building with two levels of shared underground parking. Overall, the proposed development will contain a total of 637 units. In order to permit the proposed development, Official Plan Amendment (“OPA”), Zoning By-law Amendment (“ZBLA”), Removal of Holding System, Draft Plan of Condominium and Site Plan Applications are required.

The proposed development, OPA and ZBLA

- Are consistent with the Provincial Policy Statement 2020;
- Conforms to the Growth Plan for the Greater Golden Horseshoe 2019;
- Conforms to the Region of Halton Official Plan
- Maintains the intent of the Town of Halton Hills Official Plan;

- Maintains the intent of the Town of Halton Hills Zoning By-law;
- Is compatible with the surrounding built form
- Represents good planning and is in the public interest.

Based on a review of the subject lands, the surrounding community, supporting studies, and the applicable planning policy framework, the subject applications represent good planning and facilitate an appropriate form of development for the community of Georgetown and the Town of Halton Hills.

Regards,

**Arcadis**

**Appendix A**

**Draft Official Plan  
Amendment**

**AMENDMENT NO. XX  
TO THE OFFICIAL PLAN  
FOR THE TOWN OF HALTON HILLS**

**THE CORPORATION OF THE TOWN OF HALTON HILLS**  
**BY-LAW NO. 2023-XXXX**

A By-Law to adopt Amendment No. XX to the Official Plan of the Town of Halton Hills

Whereas the Council of the Town of Halton Hills, in accordance with the provisions of the Planning Act., 1990 R.S.O., c.P. 13, as amended, hereby enacts as follows:

1. That Amendment No. XX to the Official Plan of the Town of Halton Hills, being the attached text and schedules, is hereby adopted.

By-Law read and passed by the Council of the Town of Halton Hills this \_\_\_\_\_ day of \_\_\_\_\_, 2023.

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Mayor – Ann Lawlor

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Clerk: S. Jones

**AMENDMENT NO. XXXX**  
**TO THE OFFICIAL PLAN FOR THE TOWN OF HALTON HILLS**

The attached text and schedules constitute Amendment No. XXXX to the Official Plan of the Town of Halton Hills, which was adopted by the Council of the Town of Halton Hills by By-law 2023-XXXX, in accordance with the provisions of the Planning Act, 1990, R.S.O. c.P. 13, as amended:

THE COPORATION OF THE TOWN OF HALTON HILLS

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MAYOR – Ann Lawlor

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CLERK – S. Jones



**AMENDMENT NO. XXXX**  
**TO THE OFFICIAL PLAN FOR THE TOWN OF HALTON HILLS**

**PART A-** THE PREAMBLE does not constitute part of this Amendment

**PART B-** THE AMENDMENT, consisting of the following text and schedules, constitutes Amendment No. XXXX to the Official Plan of the Town of Halton Hills.

## **PART A – THE PREAMBLE**

### **1. Purpose of the Amendment**

The applicant (1 Rosetta Street Inc.) proposes to develop the properties municipally known as 1 Rosetta Street, and 6 and 8 St. Michaels Street at a density above that identified in the Official Plan for High Density Residential/Mixed Use Area 2. Official Plan Amendment and Zoning By-law Amendment applications have been submitted to allow for a 637-unit residential development complex consisting of three (3) buildings with heights of 8 and 12 stories and an associated park.

The purpose of this amendment is to redesignate the lands on the property from High Density Residential/ Mixed Use Area 2 to High Density Residential/Mixed Use with Special Provisions which would allow the development to achieve heights up to eight storeys for building fronting on to local roads (Caroline and Rosetta Streets) and 12 storeys for buildings fronting onto railway lands (River Drive and St. Michaels Street) with a total Floor Space Index (FSI) of 3.16.

### **2. Location of the Amendment**

The amendment applies to lands shown on Map 1.

### **3. Details of the Amendment**

#### **Text Change**

Part H, Section H3.9 – SPECIAL POLICY AREAS is amended by the addition of Subsection H.3.9.X.

#### **Map Change**

Schedule H3 GEORGETOWN GO STATION AREA LAND USE PLAN has been amended to show the Special Policy Area XX Designation.

## **PART 2- BODY OF THE AMENDMENT**

### **1. Text Change**

#### **PART H, SECTION H3.9- SPECIAL POLICY AREAS:**

##### **H3.9.X Special Policy Area X**

The following policies apply to the lands designated as High Density Residential/Mixed Use area and identified as Special Policy Area X, as shown on Schedule H3 of this Plan:

##### **a) Density and Height**

Maximum FSI of 3.4 for the site. Buildings heights for buildings adjacent to Metrolinx rail lands and St. Michaels Street shall have a maximum height of 12 stories while buildings fronting onto local roads (Caroline and Rosetta Streets) will have a maximum height of 8 stories.

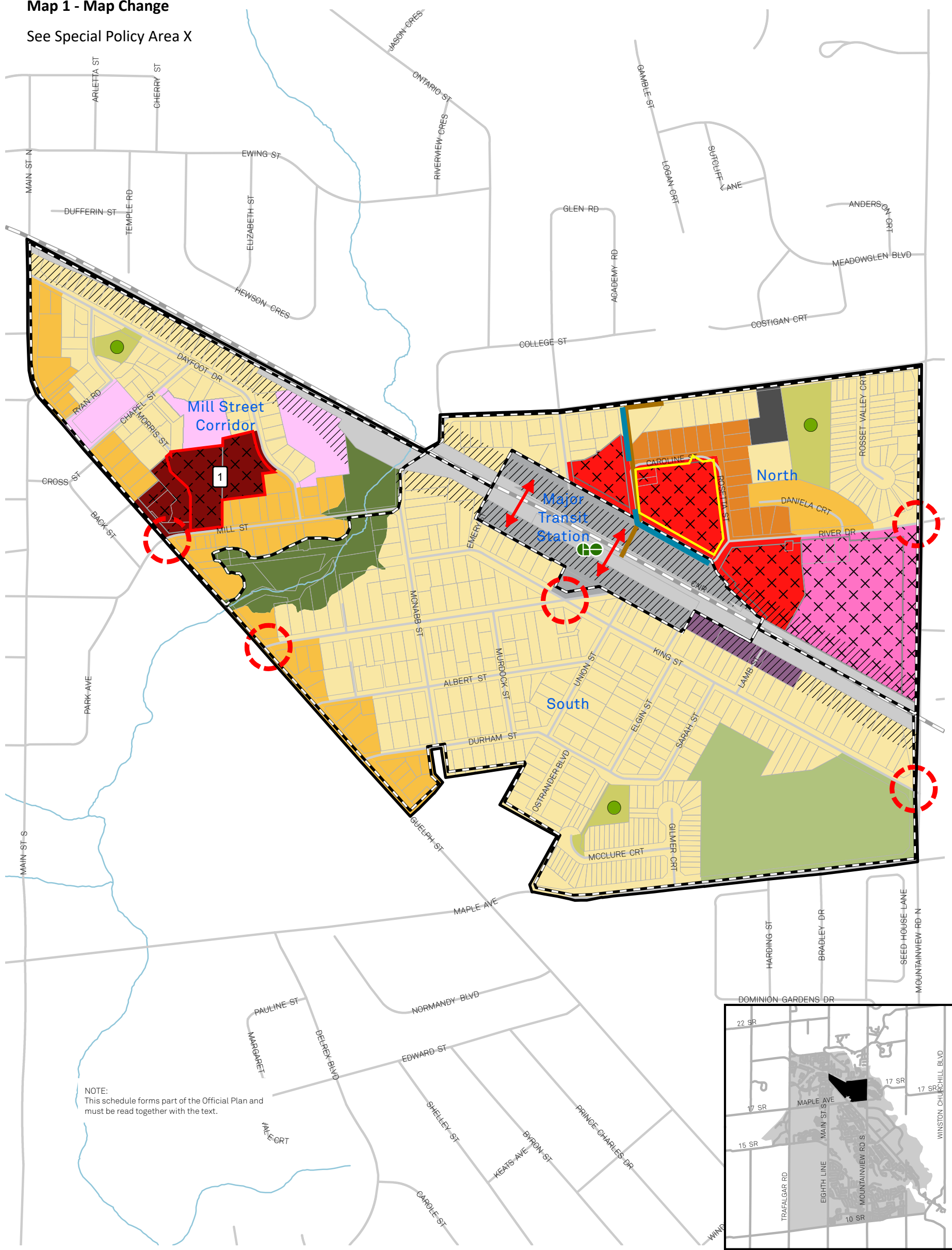
##### **b) New Development and Redevelopment Policies**

The following policies are intended to guide proposals for new development or redevelopment in Special Policy Area:

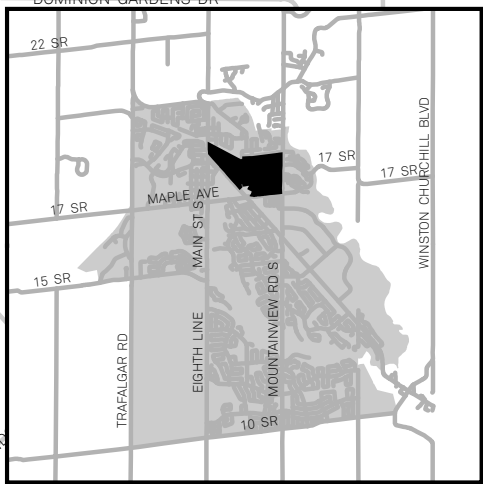
- i. New buildings are designed in a manner that is sensitive to the location, massing and height of adjacent buildings;
- ii. Any new buildings will not compromise the ability to redevelop any adjacent property;
- iii. A high standard of urban design is applied and any new building shall generally conform to the policies set out in Section F2 (Urban Design) of the Town of Halton Hills Official Plan and the urban design guidelines for the Georgetown GO Station Area Secondary Plan;
- iv. Any façade at street level adjacent to residential buildings shall incorporate broad window treatments and other architectural features and design elements to maintain an open and interesting pedestrian friendly environment. No blank walls shall be located at street level adjacent to residential uses;
- v. Blank walls or any portion of the foundation or underground garage exposed adjacent residential properties, which are due to grade changes on site shall be appropriately landscaped to provide a visual buffer or screen.





























**Map 1 - Map Change**

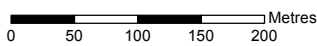
See Special Policy Area X



NOTE:  
This schedule forms part of the Official Plan and must be read together with the text.



- |  |  |  |   |
|--|--|--|---|
|  Greenlands                             |  Medium/High Density Residential Area             |  Commuter Rail Corridor     |  Gateway                                 |
|  Park                                   |  High Density Residential/Mixed Use Area 1        |  Major Transit Station Area |  Below/Above Grade Pedestrian Connection |
|  Cemetery                               |  High Density Residential/Mixed Use Area 2        |  Railway Buffer             |  Pedestrian Trail Connection             |
|  Low Density Residential Area           |  High Density Residential/Community Facility Area |  Redevelopment Site         |  Proposed Road Extension                 |
|  Medium Density Residential Area        |  Employment Area                                  |  Decision Withheld          |  Watercourse                             |
|  Medium Density Residential/Office Area |  |  Go Station Area Boundary   |  Railway Line                            |
|  |  |  Special Policy Area        |  Neighbourhood Park                      |
|  |  |  Precinct                   |  Major Transit Station                   |
|  |  |  Special Policy Area X      |   |



## **Appendix B**

# **Draft Zoning By- Law Amendment**

**DRAFT ZONING BY-LAW AMENDMENT**  
**BY-LAW NO. 2023- \_\_\_\_\_**

Being A By-law to Amend Zoning By-law 2010-0050, as amended, of the  
Town of Halton Hills, Lots 3, 4, 5, 6, 38 and 39, and Part of Lots Part of Lots 1 and 37 on Plan 29 , Lots 30, 31,  
32, 33-39 on Plan 37, municipally known as 1 Rosetta Street, Part of Lots 1 & 2, PL 29 municipally known as 6 St.  
Michaels St. and Part of Lot 2 Plan 29 municipally known as 8 St. Michaels St.  
Town of Halton Hills, Regional Municipality of Halton (“the Subject Lands”)

**WHEREAS** Council is empowered to enact this By-law by virtue of the provisions of Section 34 of the Planning Act, R.S.O.1990, as amended;

**AND WHEREAS** on \_\_\_\_\_, 2023, Council for the Town of Halton Hills approved Report No. PL \_\_\_\_\_, dated \_\_\_\_\_ in which certain recommendations were made relating to amending Zoning By-law 2010-0050;

**AND WHEREAS** Council has recommended that Zoning By-law 2010-0050 be amended as hereinafter set out;

**AND WHEREAS** said recommendation conforms to the Official Plan for the Town of Halton Hills;

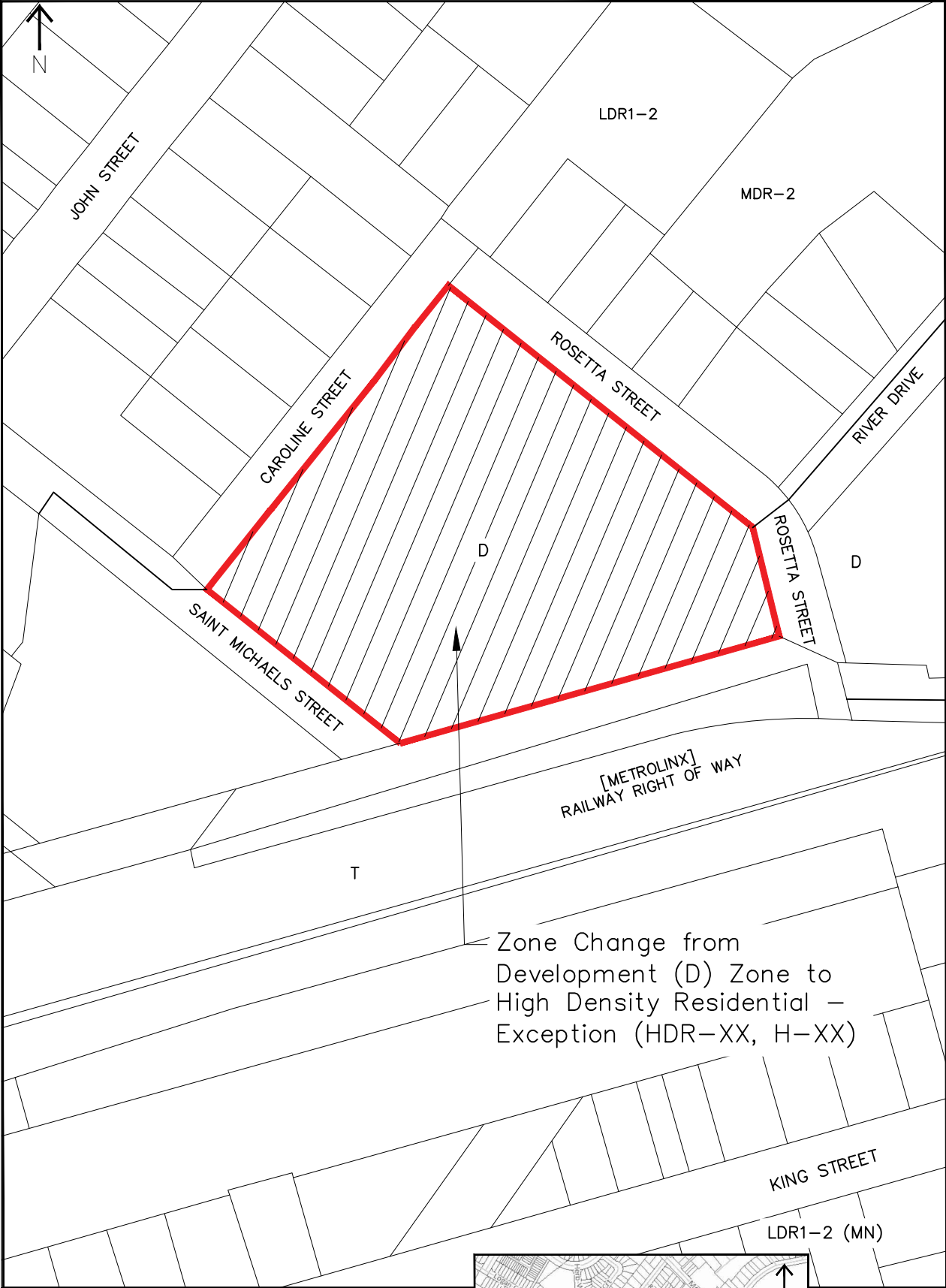
**NOW, THEREFORE, THE COUNCIL OF THE CORPORATION OF THE TOWN OF HALTON HILLS ENACTS AS FOLLOWS:**

1. That Schedule “A03-1 and A03-2-Georgetown” of Zoning By-law 2010-0050, as amended, is hereby further amended by rezoning the Subject Lands described above as shown on Schedule 1 attached to and forming part of this By-law from a Development (D) Zone to a HDR - High Density Residential – Exception as shown on Schedule 2 attached to and forming part of this By-Law.
2. That Table 13.1: Exceptions of Zoning By-law 2010-0050 is hereby amended by adding the Exception Provision contained in Schedule 2 attached to and forming part of this By-law.
3. That Part 14 Holding Provisions and Table 14.1: Holding Zones of Zoning By-law 2010- 0050 is hereby further amended by amending the Holding Provision contained in Schedule 4 attached to and forming part of this By-law.

**BY-LAW** read and passed by the Council for the Town of Halton Hills this \_\_\_\_ day of \_\_\_\_\_, 2023.

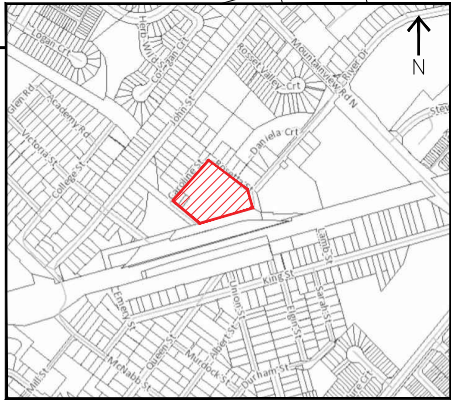
\_\_\_\_\_  
Mayor – Ann Lawlor

\_\_\_\_\_  
Clerk – Suzanne Jones



Zone Change from  
Development (D) Zone to  
High Density Residential –  
Exception (HDR-XX, H-XX)

 SUBJECT PROPERTY



**SCHEDULE 2 to By-law 2023-XXXX – DRAFT ZONING BY-LAW**

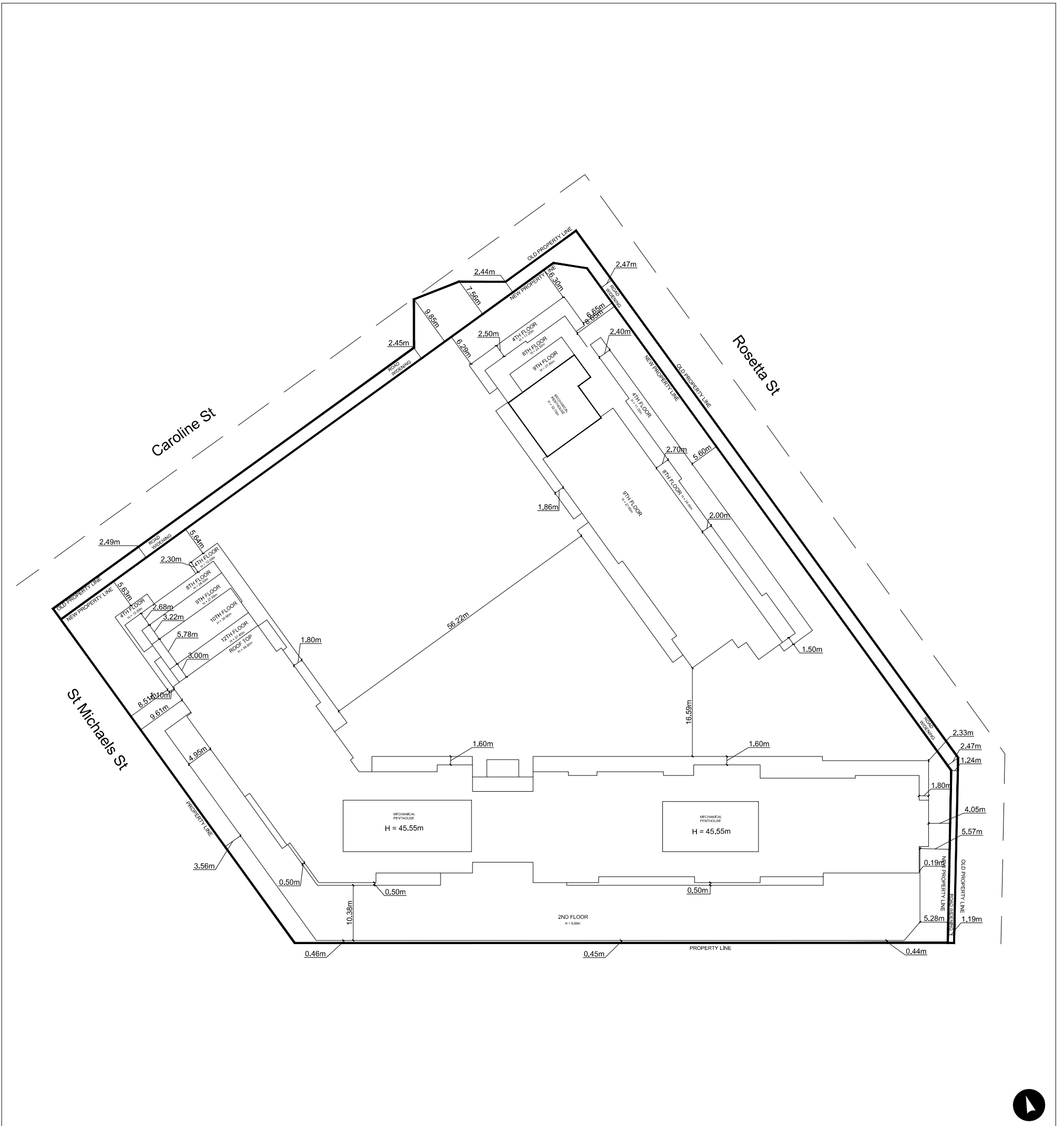
**EXCEPTIONS**

1	2	3	4	5	6	7
Exception Number	Zone	Municipal Address	Additional Permitted Uses	Only Permitted Uses	Uses Prohibited	Special Provisions
XX 2023-XXX	HDR	1 Rosetta Street, 6 and 8 St. Michaels St.				<ul style="list-style-type: none"> <li>(i) All lands within this zone are deemed to be one lot for the purposes of this Bylaw;</li> <li>(ii) Maximum height – as show on Schedule 3 to this By-law.</li> <li>(iii) No portion of the building or structure, excluding parapets, guard rails, railing and dividers, trellises, eaves, screens, stairs, roof drainage, window washing equipment, lightning rods, garbage chute overruns, plumbing vents, electrical panel with enclosure, architectural features and/or landscaping elements of a green roof is to have a height greater than the height in metres as shown on Schedule 3 of this Bylaw;</li> <li>(iv) Any mechanical features, such as structures containing a mechanical penthouse does not project more than 5.5 metres above the highest point of the roof</li> <li>(v) Maximum Number of Dwelling Units: 637</li> <li>(vi) For the purposes of this zone, Rosetta Street is defined as the front yard;</li> <li>(vii) Minimum required front yard – 2.3m (Rosetta Street)</li> <li>(viii) Minimum required rear yard (St. Michaels Street) – 3.5m;</li> <li>(ix) Minimum required interior side yard (Southern Lot Line)– 0.4 m;</li> <li>(x) Minimum required exterior side yard– 5.6 m;</li> </ul>



						<p>(xi) The minimum setback from an interior side lot line or rear lot for buildings that have a height greater than 13.4 metres does not apply</p> <p>(xii) Notwithstanding Section 5.2.11 – Minimum Width of Aisles, the minimum width of an aisle providing access to a surface parking space shall be 6.0m except as set out in Table 5.1;</p> <p>(xiii) Notwithstanding Table 5.2- Residential parking requirements - residential parking requirements for apartment dwellings shall be calculated at 1.08 space per unit plus 0.1 spaces for visitor parking;</p> <p>(xiv) Maximum number of buildings on the same lot – 3;</p> <p>(xv) Section 4.3.4- Special Railway Right of Way Setbacks shall not apply.</p>
--	--	--	--	--	--	---

# SCHEDULE 3 to By-law 2023-XXXX



NOTES:  
 1. ALL DIMENSIONS ARE IN METERS

The Height measurements shown on this schedule shall be measured from the established grade of each building to the top of parapet as per by-law:  
 established grade for Buildings 1&2 is (259.96)  
 established grade for Buildings 3 is (260.72)

**SCHEDULE 4 to By-law 2023-XXXX**

**14.1 HOLDING ZONES**

	Zone Designation	Property Description	Conditions for Removal	Date Enacted
H-XX	HDR (XX)	1 Rosetta Street, 6 and 8 St. Michaels St.	<p>The Holding (H XX) provision may be lifted upon:</p> <ul style="list-style-type: none"> <li>i. Approval of a Site Plan application and execution of a Site Plan Agreement in accordance with Section 41 of the Planning Act, for any future development;</li> <li>ii. The Town of Halton Hills and the Region of Halton being satisfied that the subject site has received the necessary servicing allocation (SDE). A Water Usage and Sanitary Discharge Report shall be submitted to the Region of Halton for review;</li> <li>iii. The Owner shall make a cash contribution to the Town of Halton Hills which shall be allocated and expended by the Town as the Owner's portion for the costs of intersection improvements at Mountainview Road North and River Drive associated with the estimated increased traffic generated from the proposed development. The amount of such cash contribution is to include but not be limited to installing left turn/right turn lanes and modifications to the existing traffic control signals.</li> <li>iv. Approval by Town Administration of a Construction Management Plan.</li> </ul>	

## **Appendix C**

# **Correspondence from Metrolinx**

**From:** [Yaniv Geler](#)  
**To:** [John Ariens](#); [Ritee Haider](#); [Reza Eslami](#)  
**Cc:** [Tommy Lee](#)  
**Subject:** FW: Georgetown GO - Rosetta Street Redevelopment  
**Date:** Monday, August 10, 2020 12:34:34 PM  
**Attachments:** [image001.png](#)  
[image002.png](#)  
[image003.png](#)  
[image004.png](#)  
[image005.png](#)  
[image006.png](#)  
[image007.png](#)  
[image008.png](#)  
[image009.png](#)  
[image010.png](#)  
[image011.png](#)

---

Hi everyone

Just FYI, no action required at this time. See below comments from Metrolinx and my responses to them.

Ritee

What is the status of the application? If you have not started yet, I can prepare it. I have all the information from the last one I did.

**Yaniv Geler**



700 Lawrence Avenue West, Suite 375  
West office Tower, Toronto ON M6A 3B4

**T 416.479.4123 F 416.479.4113 M 416.417.5702**

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**From:** Yaniv Geler  
**Sent:** Monday, August 10, 2020 12:30 PM  
**To:** Tony To <Tony.To@metrolinx.com>; Brandon Gaffoor <Brandon.Gaffoor@metrolinx.com>  
**Subject:** RE: Georgetown GO - Rosetta Street Redevelopment

Hi Tony

Thank you for your comments and responses.

The Planner at City of Halton Hills is Jeff Markowiak. His contact info is:

Jeff Markowiak, MCIP, RPP  
Director of Development Review  
Planning & Sustainability  
Town of Halton Hills  
1 Halton Hills Dr., Halton Hills, ON L7G 5G2  
Tel: 905-873-2601 ext. 2296  
[JeffMa@haltonhills.ca](mailto:JeffMa@haltonhills.ca)

1. Please refer to drawing number 14 which shows a building section along the track side. The building is actually set back 37.231 meters from the center of the first track. The podium of the parking is which is above grade is 24.131 meters from the center of the first track and the intent of the podium wall facing the track is to be designed as a crash wall.
2. Yes, this is our design intent and we will reach out to AECOM when the time comes. Would you please provide a contact at AECOM to provide the design guidelines and review our proposed design of the mitigation?
3. Confirmed. Thanks
4. Confirmed. Thanks
5. The Environmental Easement was not included. Can you please re-send.
6. Do you have a standard form agreement or is it site specific? If its standard, would you be able to send over?
7. Confirmed. Is the correct ownership name: **Metrolinx GO Transit**?
8. Confirmed.

---

**From:** Tony To <Tony.To@metrolinx.com>  
**Sent:** Monday, August 10, 2020 11:08 AM  
**To:** Yaniv Geler <yaniv@byronequities.com>; Brandon Gaffoor <Brandon.Gaffoor@metrolinx.com>  
**Subject:** RE: Georgetown GO - Rosetta Street Redevelopment

Hi Yaniv,

Apologies for the delay. Typically, Metrolinx provides comments on these types of applications directly to the responsible municipality through their planning approvals process; can I ask that you please provide me with the name of the Planner at Georgetown or Halton who is managing your application? I want to make sure that your formal submission to the municipality is circulated to Metrolinx for comment.

I have had a chance to review the materials you've provided and have noted my preliminary comments below. Please note, the comments below are preliminary in nature; additional comments from Metrolinx may be provided during the formal review process through the municipality's planning approvals process.

1. It appears the building footprint is very close to the mutual property line. As you know, typically, development is required to be set back 30 m from the rail right-of-way. Please revise the drawing to include the setback distance from the building face to the existing railway right of way (property line).
2. In accordance with Metrolinx's safety guidelines, a safety barrier designed to mitigate the impact of a potential train derailment will need to be implemented at this location. A crash barrier may be provided in the form of an earthen berm, crash wall, or a reinforced building structure. I request the plans be revised to consider a safety barrier along the southern (mutual) property line. The design of the safety barrier will need to be reviewed and approved by Metrolinx's engineering consultant, AECOM. Note, the engineering review is subject to AECOM review fees.
3. A noise and vibration impact assessment must be submitted to Metrolinx for review. You may obtain the most up to date rail forecast by submitting a request to [raildatarequests@metrolinx.com](mailto:raildatarequests@metrolinx.com)
4. A drainage study must be submitted to Metrolinx for review. Note, no additional drainage is permitted on Metrolinx-owned lands, without prior confirmation from Metrolinx and/or its engineering consultant.
5. The development shall be subject to an environmental easement. The owner shall grant Metrolinx an environmental easement for operational emissions, registered on title against the subject dwellings in favour of Metrolinx. I've attached a copy of the easement language for your information.
6. The proponent is required to enter into an Adjacent Development Agreement with Metrolinx, to ensure Metrolinx's concerns are adequately addressed.
7. Please note, the railway corridor is owned by Metrolinx GO Transit. Please ensure future drawings show the correct ownership.
8. The following warning clause shall be inserted in all development agreements/sales agreements for each unit within 300 metres of the rail right-of-way:
  - a. Warning: Metrolinx, carrying on business as GO Transit, and its assigns and successors in interest operate commuter transit service within 300 metres from the land which is the subject hereof. In addition to the current use of these lands, there may be alterations to or expansions of the rail and other facilities on such lands in the future including the possibility that GO Transit or any railway entering into an agreement with GO Transit or any railway assigns or successors as aforesaid may expand their operations, which expansion may affect the living environment of the residents in the vicinity, notwithstanding the inclusion of any noise and vibration attenuating measures in the design of the development and individual dwellings. Metrolinx will not be responsible for any complaints or claims arising from use of such facilities and/or operations on, over or under these lands.

Please feel free to contact me with any questions.

TONY TO

Project Manager  
Third Party Projects Review, Capital Projects Group  
Metrolinx | 20 Bay Street | Suite 600 | Toronto | Ontario | M5J 2W3  
T: 416.202.0809 C: 416.902.0157



---

**From:** Yaniv Geler [<mailto:yaniv@byronequities.com>]  
**Sent:** August-07-20 7:21 PM  
**To:** Tony To; Brandon Gaffoor  
**Subject:** RE: Georgetown GO - Rosetta Street Redevelopment

Hi Tony

Any updates? I thought I should have something last week from you.

---

**From:** Yaniv Geler  
**Sent:** Tuesday, July 28, 2020 11:20 AM  
**To:** Tony To <[Tony.To@metrolinx.com](mailto:Tony.To@metrolinx.com)>; Brandon Gaffoor <[Brandon.Gaffoor@metrolinx.com](mailto:Brandon.Gaffoor@metrolinx.com)>  
**Subject:** RE: Georgetown GO - Rosetta Street Redevelopment

That's right. OPA and Rezoning.

---

**From:** Tony To <[Tony.To@metrolinx.com](mailto:Tony.To@metrolinx.com)>  
**Sent:** Tuesday, July 28, 2020 8:19 AM  
**To:** Yaniv Geler <[yaniv@byronequities.com](mailto:yaniv@byronequities.com)>; Brandon Gaffoor <[Brandon.Gaffoor@metrolinx.com](mailto:Brandon.Gaffoor@metrolinx.com)>  
**Subject:** RE: Georgetown GO - Rosetta Street Redevelopment

Hi Yaniv,

Apologies for not responding sooner. Yes, I received the revised drawings and currently reviewing internally. I will aim to get you preliminary comments by end of day today or tomorrow latest. To clarify, your upcoming submission to the City is for an Official Plan amendment and rezoning?

Thanks,  
Tony

---

**From:** Yaniv Geler [<mailto:yaniv@byronequities.com>]  
**Sent:** July-27-20 6:24 PM  
**To:** Tony To; Brandon Gaffoor  
**Subject:** RE: Georgetown GO - Rosetta Street Redevelopment

Hey Tony, Brandon

Just following up on my email from a week ago, have you had a chance to review, comment, circulate internally?  
Do you want to set up a zoom call to review?

---

**From:** Yaniv Geler  
**Sent:** Monday, July 20, 2020 5:39 PM  
**To:** Tony To <[Tony.To@metrolinx.com](mailto:Tony.To@metrolinx.com)>; Brandon Gaffoor <[Brandon.Gaffoor@metrolinx.com](mailto:Brandon.Gaffoor@metrolinx.com)>  
**Subject:** RE: Georgetown GO - Rosetta Street Redevelopment

Hi Tony, Brandon

I hope you are both doing well.

Please find attached our revised plan for the project. We have included Site Plan and a building section on the Metrolinx side which shows the above grade parking garage which we will use as the proposed crash wall.

Please let me know if you have any comments, we will be submitting this plan to the city in the next few days.

**Yaniv Geler**



700 Lawrence Avenue West, Suite 375  
West office Tower, Toronto ON M6A 3B4

**T** [416.479.4123](tel:416.479.4123) **F** [416.479.4113](tel:416.479.4113) **M** [416.417.5702](tel:416.417.5702)

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**From:** Tony To <[Tony.To@metrolinx.com](mailto:Tony.To@metrolinx.com)>  
**Sent:** Tuesday, May 26, 2020 11:32 AM  
**To:** Yaniv Geler <[yaniv@byronequities.com](mailto:yaniv@byronequities.com)>  
**Cc:** Brandon Gaffoor <[Brandon.Gaffoor@metrolinx.com](mailto:Brandon.Gaffoor@metrolinx.com)>  
**Subject:** RE: Georgetown GO - Rosetta Street Redevelopment

Hi Yaniv,

Thanks for taking my call earlier. To recap, please forward the revised site plan for the Rosetta Street project that your team is currently updating at your earliest convenience. My team will have a quick look and from there we can set up a call to ensure the development doesn't conflict with Metrolinx's future plans for Georgetown GO.

Thanks and talk soon,  
Tony  
TONY TO

Third Party Projects Officer  
Third Party Projects Review, Capital Projects Group  
Metrolinx | 20 Bay Street | Suite 600 | Toronto | Ontario | M5J 2W3  
T: 416.202.0809 C: 416.902.0157



---

**From:** Tony To  
**Sent:** April-17-20 12:29 PM  
**To:** 'Yaniv Geler'  
**Cc:** Adam Snow; Reza Eslami; 'John Ariens'; 'Ritee Haider'; Brandon Gaffoor  
**Subject:** RE: Georgetown GO - Rosetta Street Redevelopment

Hi Yaniv. Thanks for your patience. My rough notes are outlined in red. When considering your approach (either option 1 or 2), I recommend we have a quick phone meeting to go over the requirements in detail, and I can provide further clarification/direction if needed.

---

**From:** Yaniv Geler [<mailto:yaniv@byronequities.com>]  
**Sent:** April-09-20 4:04 PM  
**To:** Tony To  
**Cc:** Adam Snow; Reza Eslami; 'John Ariens'; 'Ritee Haider'; Brandon Gaffoor  
**Subject:** RE: Georgetown GO - Rosetta Street Redevelopment

Hi Tony

Thank you for the call today.

I appreciate that you are looking into this and the Sponsorship group is not providing you with answers at this time. I understand that this may not be available in the near future but in the interim you will provide me with worse case scenario in terms of the track design layout. **To clarify, MX staff are currently actively working to see what information we have to share in terms of prelim design concepts for future track buildout. As discussed, as it is unclear when/if the concepts will be ready to share, it may be prudent to be conservative, or consider the "worst case scenario". I will be in touch shortly in this regard.**

We also discussed 2 options for this project in terms of phasing and waiting for the final design:

1. If we do proceed with first phase as originally planned in its current form being the buildings North of the tracks, what kind of mitigation we would have to design into the project in order to meet Metrolinx criteria for protection (i.e. crash wall and noise)
  - Minimum 30 m set back from the future rail right-of-way
  - Safety barrier, designed to the satisfaction of Metrolinx's engineering consultant, to address a potential train derailment scenario
  - Noise and Vibration Study
  - Drainage Study demonstrating any changes in grade and drainage will not negatively impact Metrolinx lands
  - 2.43m high, non-climbable and non-cuttable, security fence along the mutual property line in order to discourage unauthorized access to the rail corridor
  - Landscaping plan showing no new, tall growing vegetation will be proposed within 3.5 m of the rail corridor
2. If we proceed first with the second or third phase of the project in its current form, being the buildings on the North side of the project, what kind of design criteria would have to be met. **From this I understand you are referring to buildings 2 and 3 of the attached site plan.**
  - Minimum 30 m set back from the future rail right-of-way
  - Phasing and staging plan and timelines for the entire development
  - Safety barrier, similar requirement to Option 1, to protect Phase 2 and 3 in the event of a train derailment scenario. The barrier can be designed to address the ultimate site development, or be designed as an interim solution. Alternatively, you may choose to conduct an engineering exercise to demonstrate that the interim site condition (e.g. excessive set back distance from Phase 2 and 3 to the rail corridor, site topography, etc.) is an equivalent level of protection to the standard requirement (i.e. 30 m setback and 2.5 earth berm).
  - Noise and Vibration Study
  - Drainage Study demonstrating any changes in grade and drainage will not negatively impact Metrolinx lands
  - 2.43m high, non-climbable and non-cuttable, security fence along the mutual property line in order to discourage unauthorized access to the rail corridor
  - Landscaping plan showing no new, tall growing vegetation will be proposed within 3.5 m of the rail corridor

Also, if you could put me in touch with the Transit Oriented Group, it would be greatly appreciated. I hope they could perhaps push the Sponsorship Group along in providing the information we need since we are bringing potentially 650 units into the market which means a potential significant increase in ridership to that station. **Contact details for Rick below.**

Rick Schippling  
Director – Commercial Transactions, Business Strategy, Land Development  
[Rick.schippling@metrolinx.com](mailto:Rick.schippling@metrolinx.com)



Yaniv Geler

700 Lawrence Avenue West, Toronto ON M6A 3B4

T [416.479.4123](tel:416.479.4123) F [416.479.4113](tel:416.479.4113) M [416.417.5702](tel:416.417.5702)

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We exist to find solutions to today's most pressing challenges, from the impact of climate change to increasing urbanization and digital transformation – all with the goal of improving quality of life for people around the world. You can see this in the work we do for our clients, the opportunities we create for our people, and in our efforts to enhance the communities in which we live and work. We bring together world-class resources and the latest innovative technologies to help define the cities and experiences of tomorrow.

This has been our mission since 1888, when we were founded to transform unusable wetlands in the Netherlands into prosperous land for people to farm. And it continues today.

[www.arcadis.com](http://www.arcadis.com)

## Contact us

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